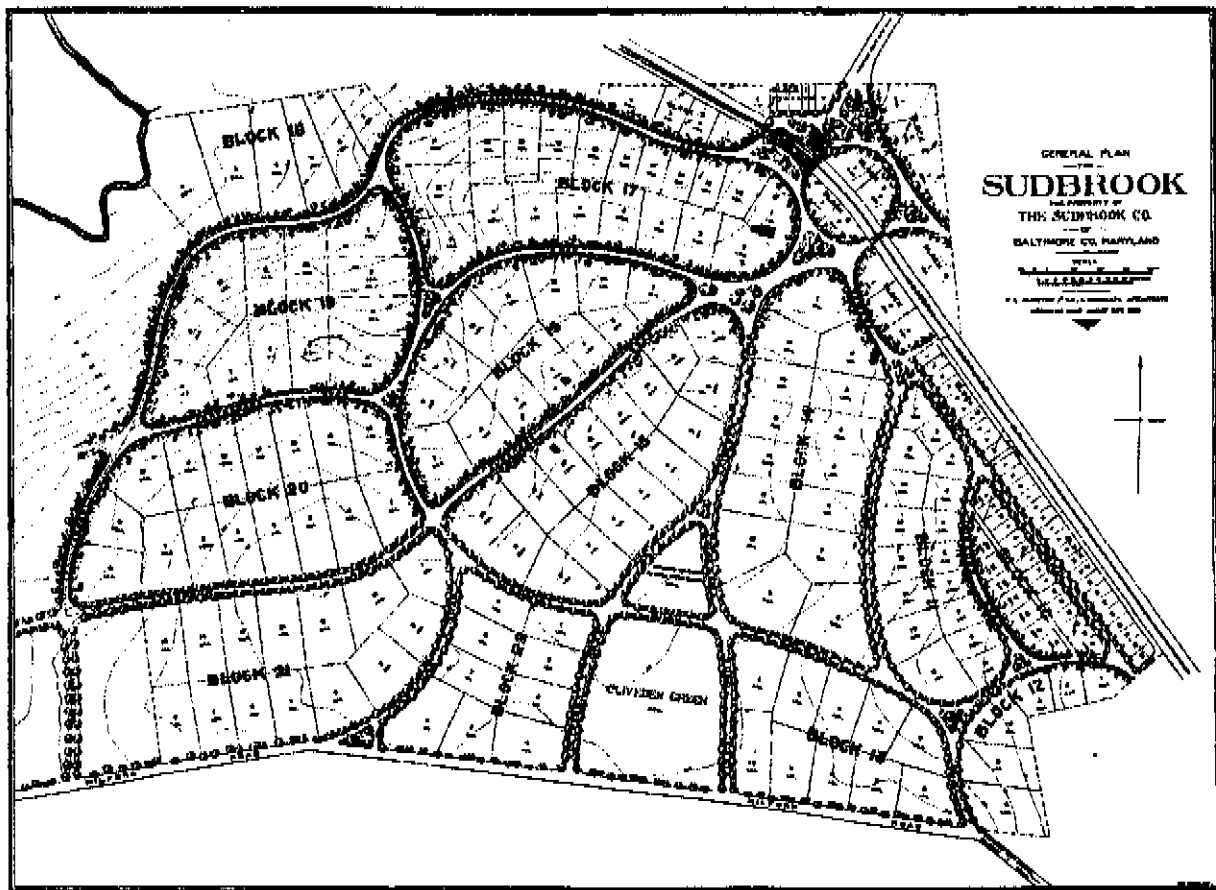


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# *SUDBROOK PARK COMMUNITY PLAN*



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As adopted by the Baltimore County Council  
April 5, 1999

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COUNTY COUNCIL OF BALTIMORE COUNTY, MARYLAND  
Legislative Session 1999, Legislative Day No. 7

Resolution No. 17-99

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Mr. Kevin B. Kamenetz, Chairman

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By the County Council, April 5, 1999

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A RESOLUTION of the Baltimore County Council to adopt the Sudbrook Park Community Plan as part of the Baltimore County Master Plan 1989-2000.

WHEREAS, the Baltimore County Council adopted the Baltimore County Master Plan 1989-2000 on February 5, 1990; and

WHEREAS, the Master Plan advocates the development and use of community plans for the established neighborhoods in the County; and

WHEREAS, in response to County Council Resolution No. 87-96, residents of the Sudbrook Park community formed an advisory group and began the preparation of an action plan for their community; and


WHEREAS, after extensive work by the advisory group, in conjunction with the County agencies specified in Resolution 87-96, the draft plan was presented to the Planning Board, was the subject of a public hearing by the Board on November 5, 1998, and was adopted by the Board on November 19, 1998; and

WHEREAS, the County Council held a public hearing on the recommended Sudbrook Park Community Plan on January 19, 1999.

NOW, THEREFORE, BE IT RESOLVED BY THE COUNTY COUNCIL OF BALTIMORE COUNTY, MARYLAND that the Sudbrook Park Community Plan, a copy of which is attached hereto and made a part hereof, be and it is hereby adopted and incorporated into the Baltimore County Master Plan 1989-2000 to be a guide for the development of Sudbrook Park, subject to such further modifications as deemed advisable by the County Council.

READ AND PASSED this *5th* day of *April*, 1999.

BY ORDER

  
\_\_\_\_\_  
Thomas J. Peddicord, Jr.  
Secretary

ITEM: Resolution 17-99

COUNTY COUNCIL OF BALTIMORE COUNTY, MARYLAND  
Legislative Session 1996, Legislative Day No. 18

Resolution No. 87-96

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Mr. Kevin Kamenetz, Councilman

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By the County Council, October 7, 1996

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A RESOLUTION of the Baltimore County Council requesting certain County agencies and community representatives to prepare a community action plan for the Sudbrook Park community.

WHEREAS, Sudbrook Park was designed in 1889 by Frederick Law Olmsted, Sr., who is the acknowledged founder of landscape architecture in this country; and

WHEREAS, Sudbrook Park is one of only three suburban villages designed by Olmsted and the only suburban design created by him in the State of Maryland; and

WHEREAS, Sudbrook Park incorporates key elements of Olmsted's design theory that should be preserved and maintained; and

WHEREAS, a portion of Sudbrook Park is listed on the National Register of Historic Sites and Places; and

WHEREAS, the Baltimore County Council created the Sudbrook Park Historic District in March of 1993 in recognition of the historic significance of the Sudbrook Park community; and

WHEREAS, the Baltimore County Master Plan recognizes and encourages local community action planning to address local, easily identifiable issues; and

WHEREAS, Sudbrook Park is a logical unit for planning within Baltimore County; and

WHEREAS, a community plan for Sudbrook Park will serve to enhance and preserve the

characteristics of the historic district and stabilize the existing community, and

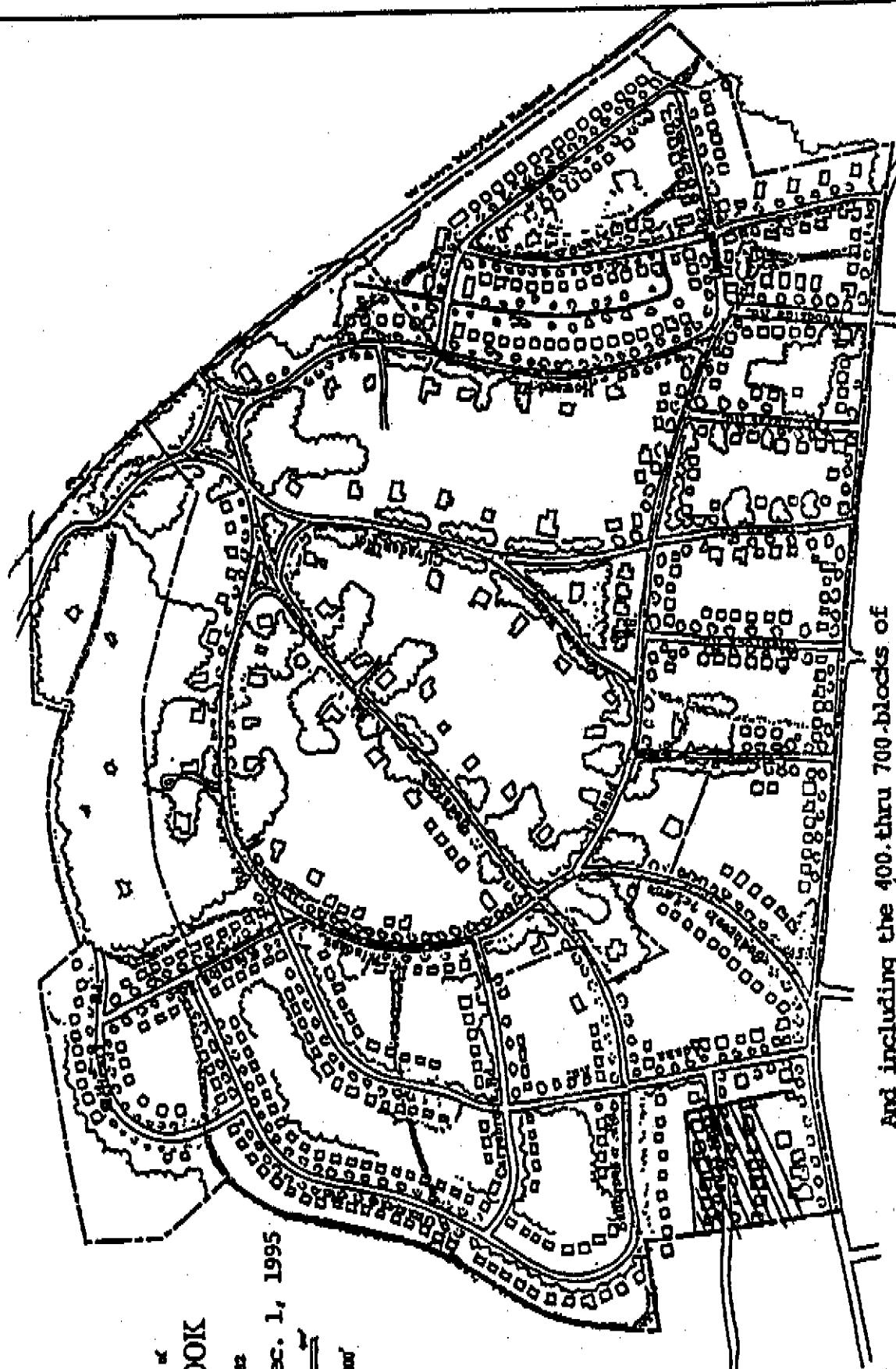
WHEREAS, there is a community and County desire to address issues relating to appropriate landscaping for the community, appropriate signage for the major access points to the community, appropriate zoning and traffic control issues, historic restoration and preservation issues, and the repair and maintenance of the historic bridge over the old Western Maryland Railroad Line; and

WHEREAS, the Baltimore County Council believes that these efforts should be continued and that these community and County agencies should solicit public input and prepare an appropriate community action plan for the Sudbrook Park Community.

NOW, THEREFORE, BE IT RESOLVED BY THE COUNTY COUNCIL OF BALTIMORE COUNTY, MARYLAND, that the Baltimore County Planning Board, in conjunction with the Office of Planning, the Department of Public Works, the Office of Community Conservation and Sudbrook Park, Inc., is requested to gather information, solicit public comment and prepare a community action plan for the historic Sudbrook Park community and submit this plan to the County Council for its approval.

R08796.

# SUDBROOK PARK COMMUNITY PLAN BOUNDARY



And including the 400. thru 700. blocks of  
Milford Mill Road

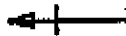
General Plan of  
**SUDBROOK**

July 24, 1962

Revised Dec. 1, 1995



Scale of 300'



## **INTRODUCTION**

### **Background**

Sudbrook Park, located in the Pikesville section of Baltimore County, was designed in 1889 by Frederick Law Olmsted, Sr., the founder of and most renowned figure in the profession of landscape architecture in this country (see Appendix A). Sudbrook Park has national, statewide and local historical significance: it is one of only three remaining intact suburban villages that Olmsted, Sr. designed in the United States and his only residential design in Maryland; it was one of Maryland's earliest planned communities; and it was the first community in the State to incorporate comprehensive deed restrictions setting forth land-use and sanitary requirements that predated Baltimore County zoning ordinances by fifty years. From its beginning to the present, Sudbrook Park has been a community imbued with history and suffused with a heritage that residents have sought repeatedly to preserve (see Appendix B).

Olmsted designed Sudbrook as a 204-acre residential community. Today, there are about five hundred families in the area known as "Sudbrook Park," which straddles Sudbrook Lane from the CSX one-lane bridge to the intersection with Milford Mill Road; the present-day community encompasses all but a small area designed by Olmsted, with the addition of the 400 through 700 blocks of Milford Mill Road, across the street from the original community's boundary (see Appendix C). About eighty to ninety acres of the community are listed on the National Register of Historic Sites and Places (also shown on Appendix C). A slightly larger area is a designated Baltimore County Historic District (Appendix D). The present-day community remains entirely residential; the Pikesville commercial district is about a mile away.

From its beginnings as an early Baltimore suburb to the present, Sudbrook Park has demonstrated a cohesive community spirit. As noted by Charles E. Beveridge, Series Editor of *The Papers of Frederick Law Olmsted* and this country's recognized pre-eminent Olmsted scholar:

*Creation of community was the great concern of Frederick Law Olmsted's pioneering career as a landscape architect. . . . In the villages and subdivisions that he planned, Olmsted . . . sought to foster a sense of community and communal life. Sudbrook, Maryland, stands as a prime example of his work in this area.<sup>1</sup>*

There has been an active community association since 1908, when the Sudbrook Company of Baltimore County relinquished daily management of the community and the Sudbrook Improvement Association was formed (the all-male group incorporated in 1942). With the encouragement of the Improvement Association, women in the community formed the Sudbrook Club, Inc. in 1946. Around the mid-1950s, the Improvement Association ceased its activities, most of which had by then been assumed by the Sudbrook Club. For over two decades the Sudbrook Club maintained an active social calendar of events and meetings, worked to insure the strictly residential nature of the community, and assumed sponsorship of holiday parades and events, some of which dated back to the earliest days of the community. In about 1973, the Sudbrook Club changed from a primarily socially-

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<sup>1</sup>From Foreword by Charles E. Beveridge to *Olmsted's Sudbrook: The Making of a Community* by Melanie D. Anson (Baltimore: Sudbrook Park, Inc., 1997), p. vii.



oriented women's club to a civic and social group open to all adults in the community. In 1996, the name of the association was changed to Sudbrook Park, Inc.

From the early 1970s to the present, the community association has been highly visible and vocal in opposing major highway, transportation and public works projects which have threatened to destroy or irreparably weaken Olmsted's design and the historic fabric of Sudbrook Park. Residents successfully defeated a six-lane expressway planned to carve through the community and achieved major victories in preserving their bridge and having the transit line partially tunneled. They are still working to preserve, through rehabilitation, the one-lane bridge with its historic function and purpose, and to restore the landscaping on the community's entranceway triangles to its "pre-transit construction" state using Olmsted's design principles. From its earliest days, the community association has also monitored zoning compliance and fought those zoning changes, both in and around the community, which were antithetical to the purely residential character of Sudbrook Park or might adversely affect the community.

For many decades Sudbrook Park residents prided themselves on being Olmsted's "hidden community" and employed a relatively reactive approach in countering challenges to the community's historic integrity and contemporary vitality. As a result, Sudbrook even today remains unknown to many Baltimoreans (see "Best Little-Known Neighborhood with Big-Name Planning" in "Best of *Baltimore*," *Baltimore* magazine, July 1995). But as residents learned when the community was threatened by the expressway and related rapid transit project, it is far more difficult to preserve a community whose value is not widely recognized. The community, identified by Baltimore County as a Community Conservation Area, was encouraged by the County to develop this Comprehensive Community Action Plan in order to publicly identify its strengths, restore and preserve its historic fabric to the greatest extent possible, obtain necessary funding, secure appropriate formal recognition of its historic significance, and set policies and goals to guide current and future governmental decisions affecting the community.

### Olmsted's Legacy to Maryland

Most community plans attempt to protect or direct the selective re-designing of the contemporary community, and if applicable, support the preservation of historic homes. The importance of Sudbrook Park's Plan rests on both of these goals . . . and one more that unites the two: to protect and renew important elements of Olmsted's 1889 Plan for the community as a whole.

Olmsted's vision found expression in an impressive spectrum of settings, from the preservation of uniquely scenic natural areas like Niagara Falls and Yosemite, to picturesque college campuses (American, Amherst, Cornell, Yale and Stanford), private estates (Biltmore in Asheville, N.C.), and suburban villages. He was America's most prominent pioneer in creating picturesquely landscaped urban parks intended not as luxuries for the wealthy but as soothing amenities that would bring together persons of all classes. Beginning in 1858 with New York City's Central Park, Olmsted went on to design major parks and park systems for cities such as New York, Boston, Chicago, Louisville, Montreal and Washington, D.C. (the grounds of the U.S. Capitol).

Simultaneously, Olmsted foresaw the need to provide a respite and relief from the "cramped, confused and controlling circumstances" of cities by creating planned suburban villages "the end of which must be, not a sacrifice of urban conveniences, but their combination with the special charms and substantial advantages of rural conditions of life." When he observed that "no great city can long exist without great suburbs," there were as

yet very few distinct suburbs, at any distance from the city. In 1869 Olmsted and Calvert Vaux designed their first suburban village, Riverside, near Chicago, which became the prototypical nineteenth-century romantic suburb and the model for subsequent early suburban development. Although the 1000-acre Riverside is much larger than Sudbrook, both communities are well-preserved examples of Olmsted's design principles. Atlanta's Druid Hills is the only other surviving suburban village attributable to Olmsted, Sr.

Olmsted's 1889 design for Sudbrook, revolutionary for its time, even today remains a work of art. In an age when arrow-straight streets formed a predictable grid of right-angled intersections, Olmsted designed roads of continuous curvature--a concept so novel that the Sudbrook Company initially could not find any surveyor or engineer to lay them out. His gracefully-curved streets were intended "to suggest and imply leisure, contemplativeness and happy tranquility" in contrast to the "ordinary directness of line in town-streets" which "suggest[ed] eagerness to press forward, without looking to the right or to the left."<sup>2</sup> Olmsted's plan for Sudbrook incorporated 6 other major design principles:

- A distinct approach and entranceway to the community, leading traffic around a tight curve and across a narrow bridge, after which five gracefully curving roads fanned out like streamers to weave through and surround the community;
- Open greenspaces complemented at intersections by landscaped triangles that served as public grounds for informal gatherings of residents;
- An abundance of hardwood trees and other vegetation designed to create a naturalistic effect in the public spaces and along all roads;
- A mix of both larger and smaller sized lots, which was almost unheard of at the time;
- The separation of distracting elements (such as the commuter train that initially served the community) using artful design and natural barriers; and
- Comprehensive deed restrictions -- the first in Maryland -- to protect the master plan, establish sanitary requirements and maintain the residential character of the neighborhood.

As one of the Baltimore metropolitan area's earliest planned suburbs, Sudbrook Park became a model for later suburban development in the region. Later communities that were designed by Olmsted's sons and the firm he founded -- such as portions of Roland Park, Guilford, Homeland, original Northwood, old Dundalk, and Gibson Island -- replicated many of the principles that Olmsted had incorporated in his design of Sudbrook. Subsequent developments in the northwest area of Baltimore County, including Dumbarton, Villa Nova, Lochearn, and Colonial Village, attempted to emulate some of Sudbrook's curvilinear roads.

Today, as suburban sprawl and subdivisions proliferate in Baltimore County, Sudbrook Park remains an example of successful community design that has stood the test of more than a century. In an age when citizens and commentators alike lament the loss of community spirit, and movements such as New Urbanism search for ways to recapture it, Sudbrook Park vividly demonstrates that Olmsted's design created an extraordinary sense of place and community that continues to nurture a strong cohesiveness among residents.

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<sup>2</sup>Olmsted, Vaux & Co., "Preliminary Report Upon the Proposed Suburban Village at Riverside, near Chicago," (New York: Sutton, Bowne & Co., 1868).

## Purpose of this Comprehensive Plan

This Comprehensive Plan for Sudbrook Park was developed under the auspices of Sudbrook Park, Inc. and Baltimore County government to identify and maintain the community's strengths, assess and address problems, define current and future goals, and recommend actions to implement these goals. Although many of the stated goals are shared by both the Sudbrook Park community and Baltimore County government, some goals and/or actions are specific to one group or the other. Goals and/or actions that are the responsibility of the community, rather than the County, are identified as such in this Plan.

Underlying principles guiding the development of this plan include Sudbrook's status as a National Register and Baltimore County Historic District (discussed previously) and its designation as a Community Conservation Area in the Baltimore County 1989-2000 Master Plan (hereafter referred to as the "Master Plan").

According to the Master Plan (p. 38), community conservation "refers to public and private efforts designed to maintain or enhance the physical, social, and economic resources of the County's older, urban area communities." In conjunction with this designation, a policy in the Master Plan states in pertinent part:

*The quality of life in existing communities should be protected by active, considerate, and cooperative efforts of County government and community improvement organizations. . . . Commercial activities incompatible with local neighborhoods should be restricted in such areas. (Master Plan, pp. 37-38).*

In addition, the current County administration has placed a high priority on preserving and revitalizing the County's older areas. As a recognized Baltimore County Conservation Area and with its historical significance now documented,<sup>3</sup> Sudbrook Park envisions a new paradigm in which the County will work actively and cooperatively with residents in responding to present and future challenges in a way that:

- recognizes Sudbrook Park as a unique suburb that can serve as a model locally and nationally;
- appropriates necessary funding to authentically preserve and/or restore Sudbrook's unique historic features;<sup>4</sup> and
- protects and ensures the continuation of the special quality of life that this community nurtures.

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<sup>3</sup>See *Olmsted's Sudbrook: The Making of a Community* by Melanie D. Anson (Baltimore: Sudbrook Park, Inc., 1997), described by the *Baltimore Sun's* James Bready as "simply the most thoroughly and intelligently detailed book yet about a Greater Baltimore neighborhood." (April 26, 1998 *Sunday Sun*, p. 14E)

<sup>4</sup>Baltimore City has authentically restored areas such as Union Square (including pink-tinted sidewalks), and in a current \$1.4 million project to improve Homeland Avenue between Charles Street and York Road, is installing a new road surface, pavements that meander around trees, speed humps, new trees and pressed concrete resembling cobblestone to enhance curbs. See the *Baltimore Sun*, "Filmmakers discover Union Square" by Marilyn McCraven, Oct. 6, 1997 (noting the economic benefits that accrued to the City when Union Square was chosen as the site for a major motion picture) and "Intersection Closes for Repairs after Water Main Break," Sept. 30, 1997, respectively. Authentic cobblestone curbing also was recently installed around one of Roland Park's large triangles.

Like all older communities, Sudbrook Park also faces challenges as it moves into the next century. Its historic and scenic roadways are being threatened by ever-increasing traffic speeds and volume; its gateway entrance and bridge may be at risk; its triangles continue to be nibbled away by new asphalt and, as an aftermath of the rapid transit line construction, they lack the intimate landscaping that Olmsted intended; its public park area has been neglected -- the recreational equipment is outmoded and historically appropriate landscaping is wanting; its water, sewer and storm drain systems need updating in certain areas; its local public elementary and high school fail to attract and retain residents; its Citizens On Patrol, while successful, requires a continuing volunteer effort; and pressures from the growth of surrounding areas periodically raise major zoning threats -- from in-fill development on individual parcels to attempts to develop air-rights over nearby Metro stations. Preserving those positive aspects that make Sudbrook Park such a desirable place to live -- which is the ultimate goal of this Plan -- will require a concerted and on-going effort by residents that is actively supported by Baltimore County.

### **Sudbrook Park's Planning Process**

This comprehensive planning process officially began on October 3, 1996, when five representatives of Sudbrook Park (the "Task Force") met with Second District Councilman Kevin Kamenetz and various Baltimore County representatives to discuss the process and major points to be addressed in a community action plan. On October 7, 1996, Councilman Kamenetz sponsored Resolution No. 87-96, which was adopted by the Baltimore County Council, requesting certain County agencies and community representatives to prepare a Community Action Plan for the Sudbrook Park community (see prefatory pages to this Plan). A subsequent meeting between the Task Force and various County representatives was held on November 19, 1996.

Meetings of the Task Force were held December 9, 1996 and January 26, 1997. After developing a list of key issues to be addressed, the Task Force formed the following seven committees:

Bridge, Roads and Traffic Committee\*

Landscaping Committee\*

Utilities/infrastructure Committee\*

\*All part of the Historic and Community Preservation Committee

Land-use and Zoning Committee

Public Safety Committee

Public Schools/Education Committee

Sudbrook Stream Valley Park Committee

The Task Force expanded into the Sudbrook Park Plan Advisory Group (PAG), comprised of the seven committee chairs and two additional members. The PAG was charged with gathering community input and developing the Plan.

The goals and actions set forth in this Plan were formed with input from the community obtained in the course of committee meetings, community meetings and survey responses. The PAG also met and worked with County representatives and engaged in extensive independent investigation in the course of preparing this Plan.

A kick-off community meeting was held on February 10, 1997 at Bedford Elementary School to explain the intent and process of developing a Plan and to seek volunteers. A brief initial survey to elicit input and ideas was distributed and collected. Over fifty persons attended, and many signed up to work on the various committees. Following this meeting, Sudbrook Park, Inc. included information in its newsletters about

the development of the Plan, extending an invitation to residents to serve on committees. The Sudbrook Park neighborhood column, appearing every fifth week in the *Owings Mills Times*, included similar information and requests for volunteers.

Committees met together regularly over the following months and the PAG met on April 12, May 7, June 3 and July 1 (County representatives also attended the latter two meetings, as well as several individual committee meetings). Each committee contributed to the development of a questionnaire which was distributed to all residents the last week of April. One hundred sixty-one residents responded to the survey (a 32% response rate, well-above the 10% "good" rate of most surveys); many wrote extensive comments.

According to the community survey conducted in spring of 1997, residents initially chose, or continue to live in Sudbrook for the following reasons:

Ambiance of trees, winding streets (i.e., Olmsted's design)	84%
Sense of community (also fostered by Olmsted's design)	57%
Nearness to friends, family	45%
Living in/near historic district, homes	44%
Convenience to job	40%
Closeness to shopping	25%
Access to transportation	20%
Quality of schools	19%
Affordability	7%
Born and raised in Sudbrook Park	4%
Other (varied responses)	10%

In an age when families have become increasingly mobile, Sudbrook Park is unusually stable: 42% of survey respondents have lived here more than twenty years; 61% more than ten years.

Another community meeting was held on October 9, 1997, to present an initial working draft of the Plan for community input and comments; these comments formed the basis of additional revisions. Following a PAG meeting on November 1, 1997, the Preliminary Draft of the Sudbrook Park Comprehensive Community Action Plan was submitted to Baltimore County for review on November 17, 1997. After a December 3, 1997 meeting of two PAG representatives with Councilman Kamenetz and Community Conservation Director David Fields, a decision was made to reformat the preliminary draft Plan to distinguish between County and community goals and actions, where applicable. The reformatted Plan was sent to Councilman Kamenetz and Charles R. Olsen, Director of Baltimore County's Public Works Department, for review on February 5, 1998.

After obtaining additional feedback from the County, a revised (April) draft of the Plan was distributed to Sudbrook Park residents in advance of a community input meeting, held on May 20, 1998 at the St. Charles School auditorium. About 80 persons attended, of which 2-3 presented disagreements with particular aspects of the Plan. Oral and written comments, submitted both during and subsequent to the meeting, were overwhelmingly positive and supportive of the Plan. Subsequently, the Plan was revised to incorporate updated information presented at the May 20 community meeting, and to clarify or amend the prior draft to respond to suggestions of residents and County agencies.

## **II. MAJOR ISSUES, PROPOSED GOALS AND ACTIONS**

### **A. Historic and Community Preservation/Restoration**

**Purpose:** Nationally known planners and historians cite Sudbrook Park as both a prototype of successful community planning and an illustrative Olmsted suburb.<sup>5</sup> The Olmstedian aspects of Sudbrook Park clearly are integral to its historic identity and continued national significance. The community is vitally interested in preserving and restoring Olmsted's plan for Sudbrook by adhering to his principles and intent regarding the entranceway bridge, road design/use and purpose, open space preservation and landscaping, triangle restoration, curbing and gutters, drainage and other infrastructure issues.

**Background:** A portion of Sudbrook Park is a National Register Historic District<sup>6</sup> and a larger portion is a Baltimore County Historic Landmark District.<sup>7</sup> (See Appendices C and D). A number of residents have expressed the hope that one or both of these Historic Districts might be expanded in the future to encompass the full 204-acres designed by Olmsted. As noted in Baltimore County's Master Plan (p. 42), "[h]istoric buildings and their settings provide a continuity with the past, establish a tangible sense of place and enhance the aesthetic environment of the County." The Master Plan also states (p. 43):

*The County should improve and expand its programs to preserve and maintain historically or architecturally significant districts as well as individual structures and their immediate surroundings.*

Sudbrook Park presents for consideration concerns whose solutions are not "standard issue," but which are singular to it as Maryland's only Olmsted community, and one of only three still-surviving in this country. Ninety-six percent (96%) of respondents to the recent survey want the Plan to advocate continuing efforts to recognize and act on the historic qualities of Sudbrook Park. It is encouraging that the Baltimore County Council and County Executive also have acknowledged the importance of preserving and revitalizing Sudbrook Park, including supporting the drafting of this Comprehensive Plan. Indeed, preservation of areas such as Sudbrook can be economically advantageous to the County as well as aesthetically rewarding<sup>7a</sup>: such historical treasures can serve as drawing cards to attract those who favor a picturesque, naturalistic ambiance over new developments which encourage urban sprawl and require additional infrastructure. Preservation of Sudbrook Park is consistent with the State's "Smart Growth" policy which proposes that older communities with existing services should be favored over new

<sup>5</sup>See, for example, *The American City, What Works, What Doesn't* by Alexander Garvin, New York: McGraw-Hill, 1996, pp. 263-266; and *Frederick Law Olmsted, Designing the American Landscape* by Charles E. Beveridge, photographs by Paul Rocheleau, New York: Rizzoli International Publications, 1995, pp. 124-25.

<sup>6</sup>As explained in the County's Master Plan (p. 43), the National Register is "a list of districts and individual properties protected through stringent reviews in the planning phases of federally funded projects." Because listing on the National Register offers protections without placing restrictions on homeowners, it is viewed as highly advantageous. [It was this listing which saved Sudbrook Park from the threatened Northwest Expressway and enabled the community to obtain concessions from the MTA regarding construction of the transit line through Sudbrook.]

<sup>7</sup>Listing as a Baltimore County Historic District carries restrictions along with its added protections. Any proposed external change, addition or demolition to a property in a Baltimore County Historic District must be reviewed and approved in advance by the Baltimore County Landmarks Preservation Commission. Because there are restrictions, there is a requirement that 75% of the property owners within a proposed district's boundary must sign a petition in order to seek nomination as a district.

<sup>7a</sup>Sudbrook Park and its houses have been featured in two Hollywood movies, "That Night" (filmed in Sudbrook Park) and "He Said; She Said." Sudbrook and one of its houses also was chosen to be the primary setting for Barry Levinson's "Avalon," but the owners declined due to the estimated length of the project.

development as a means of preserving rural areas and minimizing the cost of new services and facilities.

### **Historic Issues**

***Goals 1-4 below and related Actions are the responsibility of Sudbrook Park, Inc.:***

**GOAL 1: To promote and spread accurate knowledge about Sudbrook Park's history and historical significance.**

**Action 1a:** Correct the mistakes in the text of the existing historical marker on the Cliveden Triangle and review/reconsider the siting of the marker.

**Action 1b:** Schedule periodic community activities focusing on Sudbrook Park's history, such as presentations, exhibits, walking tours, etc.

**Action 1c:** Promote the sale of two books about Sudbrook's history and houses, the profits of which go toward the community's preservation.

**Action 1d:** Participate with other Baltimore County Historic Districts and/or Olmsted groups in joint educational efforts.

**GOAL 2: To pursue an expansion of the Sudbrook Park National Register Historic District (a listing that does not place restrictions on homeowners.)**

**Action 2a:** Re-initiate discussions with the Maryland Historic Trust on this issue and ascertain the level of that agency's support.

**Action 2b:** If there is sufficient support and community interest, prepare a new or revised nomination proposal.

**Action 2c:** Follow the necessary procedures at the State and national level.

**GOAL 3: To ascertain if there is sufficient interest to nominate additional areas of Sudbrook Park to become a Baltimore County Historic District.**

**Action 3a:** Hold community meetings to explain the restrictions and benefits of being a County Historic District.

**Action 3b:** If there is sufficient interest to obtain the necessary consents of homeowners, pursue additional nominations.

**GOAL 4: To actively encourage elected officials to (a) strengthen and expand the powers of the County's Landmark Preservation Commission and (b) increase the Commission's administrative support staff to better monitor development activity affecting historic properties and districts.**

**Action 4a:** Support the appointment to the Landmarks Commission of qualified persons with a strong background in historic preservation.

**Action 4b:** Pursue changes to Baltimore County regulations that (1) strengthen preservation efforts; (2) more clearly preserve historic landscapes and a historic structure's setting, including significant trees; (3) address violations of Historic Landmark regulations in a more timely and effective manner; and (4) continue to improve the system by which the County processes permits for work within Historic Districts to insure that the Baltimore

County Landmarks Preservation Commission automatically is notified, and the required approvals are obtained, before a permit is issued.

## **B. Sudbrook Park's Bridge, Roads and Traffic**

**Purpose:** Frederick Law Olmsted's 1889 design for Sudbrook Park roadways contained many features now recognized as helpful in calming traffic, including curvilinear streets, a constricted bridge entry, crowned roadways that were slightly recessed below the landscape, a simple and attractive turf curb/gutter system which maintained the roadway width, and planted and treed shoulders with adjoining sidewalks. Unfortunately for Sudbrook Park, the sidewalks no longer exist and the curb/gutter system is long gone. While the community has retained its attractive "rural" look with no gutters defining the edge of many of its roads, repeated re-paving has gradually widened these roads over time. This coupled with greatly increased traffic has made the use of several community roads less pleasant and even risky for walkers, runners, cyclists and all but vehicular passengers.

Also, MTA Metro construction (including a cut-and-cover tunnel) destroyed many of the old trees and plantings that helped to narrow and define the edges of Sudbrook Lane leading away from the bridge. The present "wide open feeling" when one approaches or leaves the bridge on the Sudbrook Park side encourages people to accelerate. The result is that many cars travel through Sudbrook Park at speeds well in excess of the posted speed of 25 mph. (See Appendix E, which contains four photographs showing this entranceway area as it once looked -- before construction of the rapid transit -- and how it looks today.) Paul Daniel Marriott of the National Trust for Historic Preservation, in an April 1996 report regarding the Sudbrook Bridge and its approaches, stated: "Restoration of the original Olmsted landscape with paths and nineteenth century planting schemes would further assist in making the western approach less of a open 'interstate-like' landscape."

**Background:** Olmsted designed a narrow bridge to serve as a constricted "gateway" to his community -- announcing a distinct entrance while also slowing traffic. This was in keeping with his belief that the essential qualification of a suburb was "domesticity" (home, family and neighborliness) and "all that favors movement should be subordinated." Originally wooden with a slightly bowed shape, the bridge was rebuilt as a simple metal span with wooden deck in about 1906-07 by the Western Maryland Railroad. Currently, this same bridge is owned by CSX Corporation; the County inspects the bridge and handles most of the repairs. A separate 8-foot trestle rising above and situated beside the bridge carries a 12-inch water main from Howard Road to Clarendon Ave. This trestle and the location of existing W-beams obscure the sight-line across the bridge from the Clarendon Ave. side. The County initiated plans to relocate the trestle and water main beneath the bridge almost two decades ago, but has never carried through on this project. While residents of Sudbrook Park want a safe and attractive bridge, the clear majority of residents have long opposed County efforts to widen it.

Widening the Sudbrook Lane bridge would almost certainly encourage unwanted, additional traffic that is not in keeping with Sudbrook's designation as a Community Conservation Area and a provision of the Master Plan (pp. 39-40) which states:

*"The County will consider the use of Community Conservation Area designations to facilitate stringent review of development proposals and direct attention to specific community needs. Proposals encouraging extra traffic harmful to the neighborhood should be avoided. Designated areas would also be given special attention as a part of the Capital Improvement Program." (Emphasis added).*



The community desires to preserve/rehabilitate or replicate its narrow entranceway bridge. In the recent community survey, an overwhelming majority (92%) of respondents preferred a one-lane bridge; 99% wanted any new bridge to continue Olmsted's design ideals and convey an historic character; and 98% opposed any widening or straightening of Sudbrook Lane on either side of the bridge. The community has been and is working with Baltimore County regarding the bridge. As Marriott noted in his report: "The Sudbrook Bridge has the potential to become a national model of community participation and sensitivity to an historic district."

In addition to preserving the form, function and purpose of the bridge, this Plan recommends that the paved approaches to the bridge, particularly on the Sudbrook Park side, be narrowed and the entranceway triangles enlarged and restored with more dense Olmstedian planting schemes, as existed in Sudbrook Park before the transit line construction (see Apx. E). The Marriott report recommended such narrowing, noting that "[n]arrowing the approach pavement would suggest a change in road and help to modify driver behavior."

In the design of roads, Olmsted recommended "gracefully-curved lines, generous [green] spaces [at intersections], and the absence of sharp corners." Adequate drainage was a necessity; he designed a system of turf gutters to keep Sudbrook's roads clear of water. Olmsted avoided "anything like the ordinary high curb of the town streets," and designed gutters to be "as shallow and inconspicuous as . . . can be . . . to accomplish the required duty." Roads were contoured and slightly depressed below the adjoining land to minimize their impact on the landscape and promote drainage. (See Appendix F, Olmsted's cross-sections for Sudbrook's roadways).

One of the characteristics of Sudbrook Park which both defines and differentiates it from other urban and suburban communities is its curvilinear roads with no discernible curbs. While this perpetuates a distinctly rural ambiance that residents want preserved, it offers no protection from tire-gutting and creates a situation in which each time the community's roads are repaved, the asphalt inadvertently expands farther into the green space. It is important to restore the contours and narrower widths of many Sudbrook streets, and to find a historically appropriate solution to the curbing dilemma.

Baltimore County makes special provisions for protecting rural areas and rural roads, calling for the "review of County Public Works standards for roads, lighting, sidewalks, curbs, gutters, etc. to provide public safety yet maintain existing rural character" (see Master Plan, p. 53). Sudbrook Park is not designated a "rural" area since it is within the County's urban-rural demarcation line, but it is the only County Historic District that can claim national renown for its roadways and landscape design. Sudbrook is a Baltimore County treasure and its Olmsted-designed roads, bordered by views and historic landscapes that are particularly attractive, should be designated and protected at least as much, if not more, than a rural "scenic corridor." (See Appendix G, from Second District Scenic Routes brochure, Baltimore County Office of Planning).

New legislation should be passed to permit Sudbrook Park's roads to be designated as a "scenic corridor" or as "scenic byways." (Montgomery County has similar "Rustic Roads" legislation). Such legislation recognizes that historic and/or scenic roadways "are being threatened by ever-increasing traffic volumes and other intrusions" and that such roadways are "an important economic resource that provides a gracious and enjoyable environment in which to live and work."<sup>8</sup> Protection by this type of designation would (a) support the restoration and on-going maintenance of narrower roadways, (b) allow

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<sup>8</sup>See Baltimore County Council Resolution No. 95-93 (Dec. 6, 1993), sponsored by then Councilman, now County Executive C.A. "Dutch" Ruppertsberger, III.

"improvements" only to the extent necessary while preserving significant features, (c) require utilities to make "every reasonable effort" to preserve significant features of the scenic roadway segment, and (d) institute a program to preserve and protect these resources in a manner that complements and is consistent with federal and state preservation efforts.

Although many of Olmsted's design techniques functioned as natural "traffic calming" devices, their effectiveness has been nullified where roadways have been widened and landscaping removed following construction of the rapid transit line. For safety and quality of life reasons, it is important to return Sudbrook's entry roads to their pre-transit line configurations and to consider additional traffic calming measures that will address blatant disregard of the posted 25 mph speed limit in Sudbrook Park, a large percentage of which is "cut-through" traffic using neighborhood roads which were never intended to, and cannot safely carry, a high volume of traffic traveling at speeds well above the posted limit.

On May 14, 1997, Sudbrook Park volunteers worked with representatives from Baltimore County's Traffic Engineering Department compiling origin and destination data of cars using Sudbrook Park's streets between 7-9 a.m and 4-6 p.m. The highest volume of traffic (82% of which was cut-through in the morning period and 79% of which was cut-through during the evening period), was on Sudbrook Lane between the bridge and the light at Milford Mill Road. The next highest volume was generated by cars using Greenwood Road (from the direction of Old Court Rd.) to Howard to Milford Mill Road, usually via Woodside. Another common "cut-through" involved cars avoiding the Pikesville business district/Reisterstown Road by using the Sudbrook Lane bridge (from Clarendon Ave. or Sudbrook Lane) and then immediately exiting Sudbrook Park by heading west on Greenwood Road (or vice versa, coming east on Greenwood Road into Sudbrook Park and then immediately exiting over the bridge). Also heavily traveled was a short-cut used to avoid the light at Sudbrook Lane and Milford Mill Road: cars going from Milford Mill to Adana to Sudbrook Road (or Adana to Windsor) to Sudbrook Lane.

In responding to the community survey, 91% wanted Sudbrook's Plan to address speeding throughout the community. It was a recurrent theme and many residents took time to write additional comments and identify particular problem streets. The most problematic in descending order were: Sudbrook Lane (far and above the most frequently named), Howard Road (a clear second in responses), and lesser problems with Windsor Road, Milford Mill Road, Adana Road, Sudbrook Road, Olmstead Road, Upland and Cliveden Roads, Woodside Road and Greenwood Road (off Milford Mill Rd.). Kingston and Cylburn each were mentioned only once; McHenry and Westover were not mentioned as having any speeding or traffic problems. Several residents saw a need for additional stop signs; others asked for one-way streets (specifically Olmstead, Howard and Woodside), speed humps or other traffic calming devices.

In spring '97, Sudbrook Park began an informal campaign to reduce speeding in the community. The slogan "*Drive 25, Sudbrook Alive*" was used on posters and in the community newsletter. Subsequently, bumper stickers were made and distributed. Many residents commented that the campaign acted as a reminder to them to slow down, not only in Sudbrook Park, but in other communities. While sensitizing residents to the speeding problem is a good first step, it alone will not solve the problem, especially since a substantial amount of speeding comes from cut-through traffic.

Residents asked for enforcement of the speed limit by police and the community association made several unsuccessful requests to the traffic enforcement division. To qualify for enforcement, the police must first ascertain that 85% or more of the traffic is traveling five or more miles above the posted limit. The five mile requirement applies regardless of the posted speed (whether on a community street with children or on the Beltway). Residents who had to live with daily speeding felt these monitoring attempts

often gave skewed information because a marked police car was used in full view, giving drivers warning and an opportunity to slow.

On October 17, 1997, Sudbrook Park volunteers used a County radar gun and large display board (that shows the speed to motorists) to monitor the speed of cars at a location on Sudbrook Lane (from 4-6 p.m.-overcast) near the bridge where a decrease in speed is required, and on Oct. 18 along the mid-500 block of Sudbrook Lane (from 12-2 p.m.-cloudy). Cars were clocked going from 20-48 mph; a summary of the results is as follows:

<u>Using Radar Gun With Sign Board:</u>	<u>Near bridge</u>	<u>Mid-Sudbrook Lane</u>
Percent exceeding the 25 mph speed limit:	83%	88%
Percent exceeding the speed limit by five or more miles:	51%	68%
Percent exceeding the speed limit by 10 or more miles:	17%	32%

On April 11 and 13, 1998 (from 11 a.m.-12 noon and from 8:10 - 9:15 a.m. -- weather clear on both days), Sudbrook Park volunteers were positioned in a driveway at 506 Sudbrook Lane and monitored Sudbrook Lane traffic using the County radar gun. In this location, and without the conspicuous sign board, the results were as follows:

<u>Using Only Radar Gun:</u>	<u>April 11</u>	<u>April 13</u>
Percent exceeding the 25 mph speed limit:	98%	99%
Percent exceeding the speed limit by five or more miles:	94%	97%
Percent exceeding the speed limit by 10 or more miles:	58%	64%

(The average speed was 36.11 mph; the highest speed was 55 mph.)

Residents of Sudbrook Park have long recognized and complained that the community has a problem with speeding; this view was emphatically and overwhelmingly confirmed at the May 20th Community Input meeting.

In response to a May 15 request for speed enforcement, Baltimore County police were on Sudbrook Lane and Howard Road at various times from May 16 to May 27. Over approximately 21 hours, police gave out 83 tickets and 28 warnings. The police deemed speeding in Sudbrook significant enough to warrant their monitoring it periodically.

Police monitoring of speeding is important and can reduce speeds temporarily, but unfortunately its results are not long lasting. In addition to periodic enforcement, this Plan recommends the consideration and use of appropriate and proven traffic calming measures based on the primary principles that streets are not just for cars and residents also have rights.<sup>9</sup> The two types of techniques used to reduce the speed of vehicles on roadways are active and passive controls. Active physical controls, which change driver behavior and are therefore largely self-enforcing, include such things as speed tables (also called speed humps; these are not the narrow speed bumps found in parking lots), rumble strips, median barriers, chokers, narrowing of road widths and traffic circles. Passive controls are such things as stop signs, signs specifying local or restricted access (i.e., during certain hours), truck restrictions, traffic signals and pavement markings. Drivers can more easily violate passive controls; they are most effective where either compliance can be expected or enforcement is possible.

<sup>9</sup> Although there are hundreds of articles and studies that reiterate these same principles, the information quoted in this Plan about traffic calming is from Cynthia L. Hoyle, "Traffic Calming," American Planning Association Report No. 456.

Traffic calming measures have been successfully implemented for decades in England, certain European countries and Australia. Hundreds of studies over many years prove that these measures work -- increasing the safety of pedestrians and motorists. Although slower to adopt these measures, many American cities (including Baltimore City and Howard, Montgomery and Anne Arundel Counties) have now embraced this concept, realizing that improving the quality of life in residential areas often means allowing neighborhoods to reclaim their streets by making them safe for pedestrians and bicyclists. As noted in a report on traffic calming:<sup>10</sup> "Present travel habits were not formed in a vacuum. Nor are they inevitable. They are the results of choices and policy decisions by past and present governments and councils." As also noted in this report: "The volume of traffic . . . is not something like the rainfall that has to be accepted."

While many Sudbrook Park residents have requested speed tables/humps to calm traffic (especially on Sudbrook Lane and Howard Road), and their effectiveness has been shown in repeated studies, this Plan recommends adding speed humps in Sudbrook Park only as a last resort, if other methods to calm speeding fail or are not feasible. At this time, Baltimore County Traffic Engineering is still developing a policy regarding the installation of speed humps, and is not using this method of traffic calming. The Bridge, Roads & Traffic Committee looked into the experience of several other Maryland jurisdictions that do use speed humps, talked with their traffic engineering departments, and inquired about the effect of speed humps on snow plows and emergency vehicles.

A "Speed Hump Program Evaluation Report" prepared for presentation to the Montgomery County Council, January 1998, considered the effectiveness and impact of speed humps and certain other traffic calming and enforcement methods. As of that date, 1,146 speed humps had been installed on over 300 Montgomery County roads since 1994. (That county currently has a three-year waiting list of communities requesting speed humps.) The Report states (p. 7) that "[i]n terms of snow removal, no additional difficulty or costs are associated with speed humps" and concludes that of "the available traffic calming tools, speed humps have proven to be the most effective and least costly way" of reducing vehicle speeds (p. 14). Comparing the negative aspects of having speed humps ("a slight increase in the response time of emergency equipment<sup>11</sup> and the perception of some residents that they are an unfair impediment to travel on public streets," p. 14, along with some shift of traffic to non-humped streets) with the positives (reduction of vehicle speeds, general lowering of accident rates, improved air and noise quality, improved quality of life along streets with speed humps), the Report concludes that "overall, the addition of speed humps to our roadway network has had a positive effect on traffic operations" and "the benefits outweigh the costs" (p. 14). Phone conversations with traffic engineers for Howard County produced similar comments.

In summary, it is important for safety as well as quality of life and historic reasons to preserve Sudbrook's narrow entranceway bridge and to narrow and re-landscape the bridge approaches within Sudbrook Park, which will involve the following actions if feasible:

- pass legislative measures to designate Sudbrook's Olmsted-designed roads as "scenic roads"
- restore their original contours, as practical

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<sup>10</sup>Hoyle, Myths of Traditional Traffic Planning, p. 3.

<sup>11</sup>Delays depended on a variety of factors, including type of equipment, driver speed, type of traffic calming device, etc. Montgomery County believes that "it must be left to individual neighborhoods to decide between the potential for minimal delays in emergency response times and existing problems of vehicular speeding." Report, p. 15.

- restore existing triangles and implement those proposed by Olmsted but never built
- find historically appropriate solutions to stop the erosion of greenspace along Sudbrook's uncurbed roads
- implement comprehensive traffic calming measures to reduce or eliminate speeding and decrease the volume of traffic within the community.

The Baltimore County Master Plan proposes that "a significant proportion of the Capital Improvement Program should be dedicated to physical improvements in the older neighborhoods." To supplement County capital improvement funds, Sudbrook Park, Inc. should investigate its eligibility for funds from programs like TEA-21 (Transportation Equity Act for the Twenty-First Century), the National Trust for Historic Preservation, and Preservation Maryland or other funding sources to restore infrastructure to standards or designs not traditionally performed by Baltimore County Department of Public Works.

All proposed improvements or changes to the infrastructure contained in this Plan are subject to considerations regarding safety, practicality and feasibility. The community understands that all such determinations ultimately must and will be decided by the County.

The community urges that any infrastructure changes, improvements or repairs implemented by the County be done in the spirit of the Plan and with sensitivity to Sudbrook Park's historic design and significance. In light of the identified concerns and the responses of residents, this Plan proposes the following:

#### The Sudbrook Lane Bridge

**GOAL 1:** To preserve the entranceway bridge into Sudbrook Park as a narrow, one-lane thoroughfare for reasons of safety as well as for historic, traffic calming and "quality of life" reasons.

*Sudbrook Park, Inc. will take the following actions to assist in meeting Goal 1:*

**Action 1a:** Work to insure that either the existing bridge is rehabilitated or, if a replacement bridge is necessary, that it remain a single lane bridge not readily convertible to a two-lane bridge and retain the purpose, function and form intended by Olmsted.

**Action 1b:** Take an active role in discussions and decisions with elected and appointed officials, the County and its engineering consultants regarding any rehabilitated or replacement bridge.

**Action 1c:** Retain, if desired, a historic bridge consultant to review the Baltimore County consultant's report/recommendations and to advise the community.

**Action 1d:** Meet with the Ralston and Pikesville Township communities to elicit their input.

**Action 1e:** Participate in reviews and meetings of any bridge re-design by the Baltimore County Landmarks Preservation Commission.

**Action 1f:** Seek outside historical advice and support as needed.

**Action 1g:** Keep the community apprised of developments and encourage widespread community activism.

**GOAL 2a:** To restore the approach to the bridge on the Sudbrook Park side to its pre-MTA construction status by narrowing the roadway and increasing the density of roadside plantings, improving safety for pedestrians and motorists by operating as an active traffic calming measure.

**GOAL 2b:** To preserve the Olmsted-designed Sudbrook Lane curvilinear approach to the bridge on the Reisterstown Road side, for reasons of safety as well as for historic and traffic calming reasons.

*Sudbrook Park, Inc. will take the following actions to assist in meeting Goals 2a and 2b:*

**Action 2a:** Work with private and/or the County's designated landscape architects to obtain more intimate and Olmstedian plantings near the bridge and on entranceway triangles.

**Action 2b:** Work with the County's designated engineers to narrow the approach road to the bridge on the Sudbrook Park side.

**Action 2c:** Insure that all approaches to the bridge adhere to Olmsted's design and are included in any comprehensive bridge restoration or replacement project.

**GOAL 3:** To improve the visibility and sight distance across the bridge.

**Action 3:** Baltimore County will remove current obstructions to visibility across the bridge, particularly from the Clarendon Avenue side, by choosing an alternative to the existing W-beams and relocating or redesigning the trestle carrying the water main.

**GOAL 4:** To address violations of posted weight limits on the bridge.

**Action 4:** Until the bridge is upgraded with capacity for additional loads, Sudbrook Park, Inc. will work with the County to find ways to enforce the posted weight limits.

#### Sudbrook's Roads and Roadways

**GOAL 5:** To return, wherever feasible, to the designs and intentions for roads and road-right-of-ways that Olmsted created over a century ago, adapting them as required for modern necessity and without removing or damaging existing roadway trees.

**Action 5a:** Use Olmsted's right-of-way cross-sections for Sudbrook as a model for road contours, which should be slightly crowned in the center and sunk below the "natural grade" of the abutting land on each side, as intended by Olmsted. (See Appendix F). Such contours function to improve drainage and actively calm traffic/improve safety.

**Action 5b:** Sudbrook Park, Inc. will work with the County regarding an appropriate paving material, which should be rough-surfaced (e.g., tar and chip, exposed aggregate finish) for a more "rural" look and to promote traffic calming and safety.

**Action 5c:** Re-install, as practical, Olmsted's turf gutters (adapted to accommodate modern automobile dictates) primarily along all existing and new triangles, and secondarily consider them along roadways that have no curbs, as practical; these would be aesthetically pleasing, historically accurate, and actually provide more effective drainage

than modern concrete curb and gutter. Installation must be carried out so as not to damage or require the removal of existing trees that border the roadways.

**Action 5d:** Use a porous paving system to line the Olmsted-style turf gutter, such as those manufactured under the names "Grasscrete," "Geoblock" or "Grasspave." (See Appendix H). These open, checkerboard-style systems can be formed to structurally reinforce the new turf gutters in their original configuration and dimensions, allowing the turf grass to grow between either precast concrete or dense plastic components laid as a mat just below grade. Such systems are designed to easily support modern car parking in a structurally stable environment without damaging or deforming the turf and grade. The addition of a new sub-surface storm water system where necessary, with inlets spaced down the turf gutter, would prevent runoff from eroding this construction.

**Action 5e:** When curbing is needed on uncurbed roads and the installation of turf curbs/gutters is impracticable because of existing trees, etc., Sudbrook Park, Inc. and the County will work together to select historically appropriate and acceptable alternatives to standard right-angle concrete curbing.

**Action 5f:** Obtain County funding to achieve these goals.

**GOAL 6:** To consider for safety reasons the restoration of the original sidewalks and planting spaces on the southeast side of Sudbrook Lane (Nos. 501-517). [See "Landscaping" section.]

#### Traffic Calming

**GOAL 7:** To lessen the negative impact of vehicular traffic on the quality of life of the residents of Sudbrook Park.

**Action 7a:** Sudbrook Park, Inc. will work with the County to reduce speeding on all streets in Sudbrook Park, especially Sudbrook Lane, through the most effective mix of the following initiatives:

- (1) Return Sudbrook Lane approaching the bridge to its previous narrower width and install more dense, Olmstedian plantings along the roadway edges, thus creating a "Jersey barrier" effect that tends to slow traffic; investigate whether any other road narrowings are needed or advisable.
- (2) Return existing community triangles to their original size and increase the density of plantings; include evergreens, such as had been part of the landscape in the entranceway area (see Appendix E).
- (3) Restore landscaped triangles that were planned at the intersections of Windsor/Adana and Kingston, and Howard and Upland. Consider adding triangles or traffic circles at Sudbrook Lane/Windsor and Sudbrook Road (if room permits) and Greenwood at Westover.
- (4) Consider converting to four-way stops the intersections at Sudbrook Lane/Sudbrook Road/Windsor Road, and at Sudbrook and Adana Roads. Reposition the stop signs at Howard and Woodside at Upland.
- (5) In conjunction with an analysis of data from the May 1997 traffic origin-and-destination study conducted by Baltimore County and Sudbrook Park residents, consider the use of traffic control devices and methods at the borders of Sudbrook Park (i.e., signs,

one-way streets during certain time periods) as solutions to cut-through traffic that often creates problems related to speeding.

(6) Before any streets within Sudbrook Park are considered for conversion to one-way, ascertain the level of community support, recognizing that speeding tends to *increase* when traffic is free to move in only one direction and that conversion of streets to one-way must be comprehensively planned.

(7) When considering the creation of new one-way streets, whether within Sudbrook Park or in neighboring communities, Baltimore County should conduct a comprehensive study of traffic patterns that takes into account adjoining neighborhoods; communities directly affected must have an opportunity for meaningful input.

(8) Investigate using rumble strips, such as the State Highway Administration (SHA) installed before a curve on Green Spring Valley Road, to alert motorists to a potentially dangerous situation if they do not slow down around the S-curves at Greenwood Road and the Sudbrook Lane S-curve approaches to the bridge. (Patterned sections of rough pavement (rumble strips) "cause a slight vibration in the car, which causes the driver to become more alert and/or to slow down. Studies have shown the effects of a change in road surface on speed to be mainly at the upper end of acceptable speeds in residential areas."<sup>12</sup>)

(9) If other measures are insufficient, consider the installation of speed humps for roads where the speed limit is consistently violated (i.e., Sudbrook Lane and Howard Road), using minimal accompanying signage, such as the speed humps used in Montgomery County, those recently installed by Baltimore City on Wyndhurst Road (between Roland Ave. and Charles Street) or the system presently in place at Valley Mede in Howard County (see Appendix I). (A speed hump must be long enough for both the front and rear wheels of a car to be on top of the table at once, meaning it should be 8-12 feet long and no more than five inches high. Speed humps can be comfortably crossed at 15 to 25 mph.<sup>13</sup>)

**Action 7b:** Seek continued active, periodic enforcement by the Baltimore County Police of the posted speed limit, the existing prohibition against trucks greater than 3/4 ton as through-traffic in Sudbrook Park, and bridge tonnage restrictions.

**Action 7c:** Institute procedures, or seek new legislation, to classify Sudbrook Park's roads as scenic by-ways or as a Baltimore County scenic corridor.

**Action 7d:** Sudbrook Park, Inc. shall consider consulting a traffic consultant versed in historic residential communities and the mitigation of traffic problems to formulate both short and long term solutions to traffic problems within the community.

**Action 7e:** Sudbrook Park, Inc. shall continue researching other historic and residential communities (with emphasis on the United States and Maryland) to determine the applicability of their approaches and solutions to Sudbrook's traffic issues.

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<sup>12</sup>Hoyle, p. 10.

<sup>13</sup>Hoyle, p. 10.



### **C. Landscaping, Streetscaping and Triangles**

**Purpose:** Given Sudbrook Park's heritage and status as one of only three remaining and well-preserved Olmsted planned suburban villages, the landscaping of Sudbrook's streets, triangles and park/playground is a key and central element of this Plan. [As shown on the map of Sudbrook included in this Section (p. 21a), the triangles will be referred to as follows: A = the bridge triangle, containing the Sudbrook Park sign; B = the small Howard Road triangle; C = the central triangle with our community bulletin board; D = the Christmas tree triangle; E = the Cliveden Road triangle; F = the small Upland/Sudbrook Lane triangle.] Using Olmsted's design principles, the community proposes to develop landscaping guidelines for its public spaces, restore landscaping on Sudbrook Park's public triangles and open spaces (including the Sudbrook Stream Valley Park), recapture the original size of triangles and install those never implemented, pursue adding a pedestrian walk path on southeast side (501-517) of Sudbrook Lane, and investigate possible historically appropriate alternatives to the existing standard street lighting.

**Background:** A number of concerns must be addressed. There is no grade separation between the triangles and the street, so special curbing solutions must be used to combat the problem of automobiles parking on and driving over the triangles. The bridge triangle, which was excavated during the construction of the subway tunnel, has such poor replacement soil that tree and shrub plantings over the past ten years have not survived. New topsoil, amended subsoil and an irrigation system or a water supply line are needed before any further funds are wasted on landscaping of the bridge triangle.

While additional tree planting on triangles C and E (the central and Cliveden Road triangles, respectively) has greatly enhanced their appearance, none of the triangles has benefited from planting plans which reflect Olmsted landscape design principles (which would include more hardwood trees, naturalistic groupings of understory plantings, ground cover planted around trees, and no flowering annuals or perennials, which Olmsted felt detracted from the cohesive landscape that was his signature. See Appendix J). The size of the triangles has been reduced by street resurfacing and widening. Some triangles that appear on the original Sudbrook plan were never set aside as public spaces; for example, Olmsted intended there to be a triangle at what is now the intersection of Kingston, Adana and Windsor Roads, but it was never built.

Along the road-sides and in public spaces, Olmsted directed the planting of large trees at regular intervals; graveled sidewalks also were important amenities so that residents could mingle and enjoy the scenery. Despite successful tree planting campaigns over the past five years, there are still streets within the Park which are losing or have lost their tree canopy and need to be replanted and replenished with appropriate street trees.

In addition to improving the landscaping of public spaces, this Plan addresses pedestrian enjoyment of that landscape. Although some areas constructed in the 1940s and 1950s do have paved walkways, walk paths in the older area of Sudbrook have been subsumed by lawns over time. The loss of the original sidewalks within the boundaries of the historic district is a concern, especially along the 500-520 block of Sudbrook Lane, where high speed vehicular traffic virtually eliminates pedestrian usage. As a result, this street has developed less neighborly cohesion than other streets. A majority of residents on the southeast side (Nos. 501-517) of Sudbrook Lane have expressed interest in restoring a walking path on their side of the street; this would need to be designed to meander around existing trees and other plantings or impediments. Early photographs of Sudbrook Park show a walk path through triangle C, the central triangle; this Plan suggests pursuing the restoration of this path, linking it with triangle A, the bridge triangle.

## Olmsted Landscape Design Principles

**GOAL 1: To develop landscape design principles and guidelines for public spaces in Sudbrook Park based on those of Olmsted, as reflected in his writings, his plans and his cross-section drawings for Sudbrook and similar communities.**

**Action 1a:** Sudbrook Park, Inc. will develop Olmstedian landscape design guidelines and a species list to be mandatory for Sudbrook's public triangles and open spaces, as well as voluntary guidelines and species lists for interested home owners. (See Appendix M.)

**Action 1b:** Sudbrook Park, Inc. will have these guidelines and lists reviewed by Olmsted scholar Charles Beveridge, and one or more landscape architects familiar with Olmstedian theory.

**Action 1c:** Sudbrook Park, Inc. will invite relevant speakers and will provide information periodically in its newsletter regarding Olmsted's landscaping principles, general landscaping advice and tips, environmental issues and other related topics that may be of interest to residents.

## Tree Plantings along Streetscape

**GOAL 2: To plant approximately 200 hardwood trees, including oak, elm and certain maples, in treeless planting spaces along all Sudbrook Park streets.**

*Sudbrook Park, Inc. will take the following actions to achieve Goal 2:*

**Action 2a:** Survey and document existing streetscape trees.

**Action 2b:** Develop a phased landscaping plan for streetscaping throughout the community.

**Action 2c:** Work with residents to ascertain those interested in participating.

**Action 2d:** Allocate \$500/annually from its budget to purchase five to ten mature saplings from reputable local nurseries each year, seek matching county funds, and work to get tree-maintenance agreements with adjacent home owners who wish to participate in restoring the streetscape canopy.

## Triangle Plantings

*Sudbrook Park, Inc. will work with Baltimore County to achieve the following:*

**GOAL 3a: To develop planting plans for each existing and proposed triangle, using Olmsted landscape design principles.**

**GOAL 3b: To promote resident enjoyment and care of new plantings in entranceway triangles A and C (the bridge and central triangles) by restoring the pedestrian pathways that were there originally.**

**Action 3a:** Obtain funding to implement the actions below.

**Action 3b:** Work with County engineers to determine the most feasible approach to providing a water supply for triangles A, C and E (the bridge, central and Cliveden Road triangles) and install a water hook-up or system.

**Action 3c:** Coordinate with the MTA to replace the top soil and amend the subsoil at triangle A, the bridge triangle.

**Action 3d:** Organize a design charrette to devise appropriate planting and general landscaping plans for all triangles; include representatives of Sudbrook Park, Inc. and one or more Baltimore County Landscape Architects, as well as other knowledgeable persons such as the State Forester, the Maryland Cooperative Extension Service, and private landscape architects familiar with Olmstedian principles.

**Action 3e:** Purchase and install new triangle shrubbery and trees. Enlist neighborhood volunteers to assist with plantings.

**Note:** Plantings should take place after specialized curbing (see the previous "Roads" section) has been installed to protect triangles and should be coordinated with any bridge or road-width restoration/traffic calming construction work.

#### Triangle Protection

**GOAL 4:** To reclaim original triangle dimensions (in-fill soil and landscaping may also be necessary) and protect triangles using specialized turf curbs and gutters around the perimeter of each existing and proposed triangle (see "Roads" section).

**Action 4:** Obtain funds sufficient to install specialized triangle curbing and any necessary in-fill soil and landscaping.

#### Addition of New Triangles

**GOAL 5:** To add triangles of varying sizes (as intersections permit) at Upland and Howard, Kingston/Adana/Windsor, and Windsor/Sudbrook Rd./Sudbrook Lane. Design triangles to act as traffic calming devices.

**Action 5:** Sudbrook Park, Inc. will work with Baltimore County regarding the design and installation of these three new triangles (see Appendix K -- locations of proposed new triangles).

#### Pedestrian Pathway - Sudbrook Lane

**GOAL 6a:** To use historically appropriate surfacing materials and re-install the pedestrian path and adjacent planting strip that existed on the southeast side of the 501-517 block of Sudbrook Lane (in conjunction with safety concerns and traffic calming plans for Sudbrook Lane and the broader community -- see "Roads" section).

**GOAL 6b:** To connect the Sudbrook Lane path to a restored pedestrian path in triangles C and A, the central and bridge triangles.

**Action 6a:** Work with property owners in the 501-517 block of Sudbrook Lane, and with Baltimore County, to arrive at a workable plan for a walk path that will meander around existing trees and impediments; explore narrowing Sudbrook Lane slightly, if needed.

**Action 6b:** Organize a design charrette to arrive at a final plan.

**Action 6c:** Use soft sidewalk material, such as finely crushed stone, oyster shell or slag, with a base layer (weed barrier) constructed to prevent unwanted growth.

**Action 6d:** Augment the turf planting strip between the road and the path with new tree planting, to emulate the current old-growth in Sudbrook Park.

**Action 6e:** Obtain funding from the County, supplemented by grants if necessary, to create the Sudbrook Lane/triangles path.

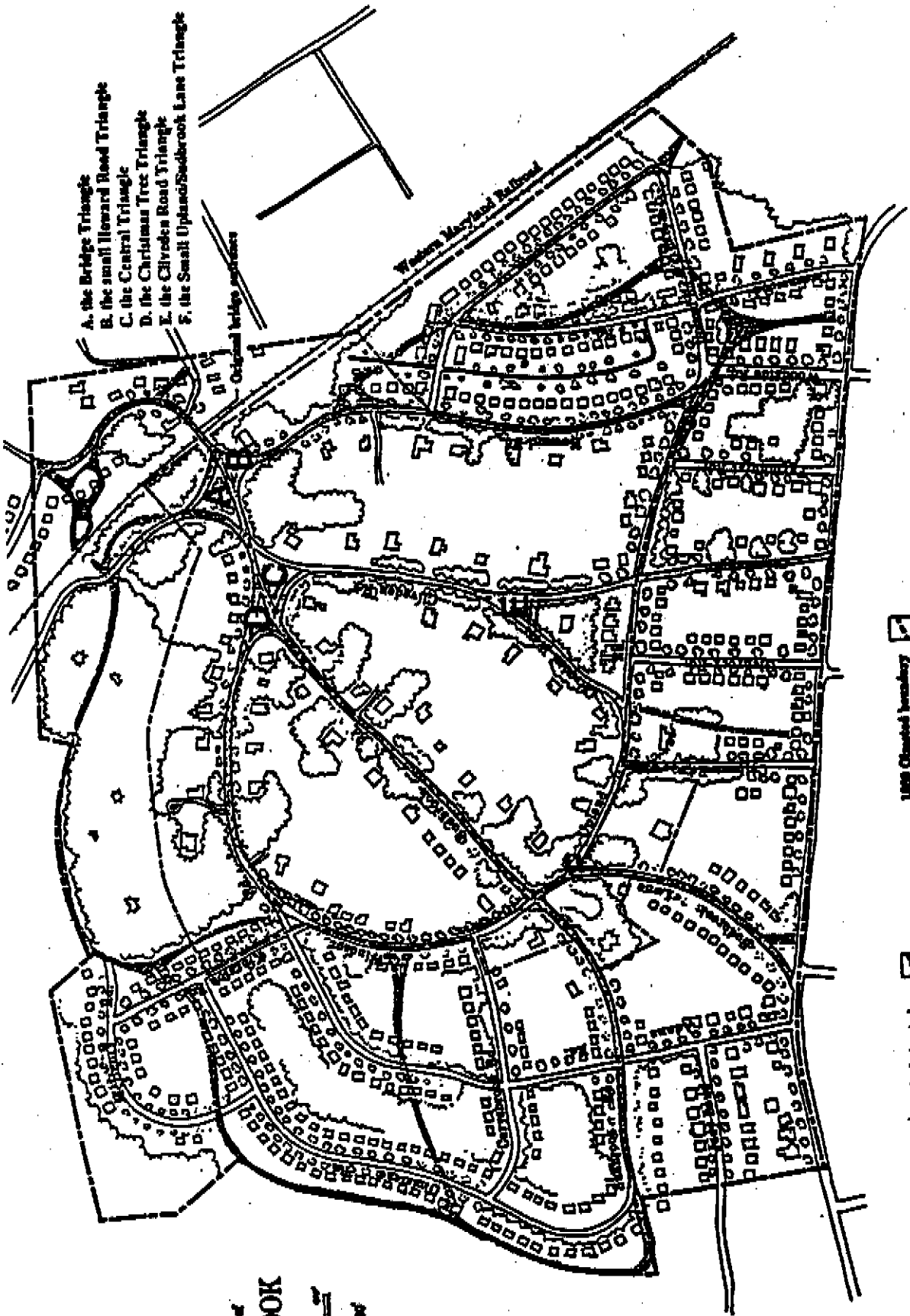
**GOAL 7: To investigate the feasibility of substituting more historically appropriate public street lighting throughout Sudbrook Park.**

**Action 7a:** Sudbrook Park, Inc. will work with BGE to obtain general cost estimates and to evaluate the feasibility of a change in street lighting.

**Action 7b:** If such an option appeared feasible, Sudbrook Park, Inc. would work with BGE, the County, residents and any private vendors to propose and implement an alternative, historically appropriate lighting scheme.

# Sudbrook Park Public Triangles

- A. the Bridge Triangle
- B. the small Howard Road Triangle
- C. the Central Triangle
- D. the Christmas Tree Triangle
- E. the Cliveden Road Triangle
- F. the Small Upland/Sudbrook Lane Triangle



General Plan of  
**SUDBROOK**  
 Scale 1" = 100'  
 1967

- 1888 Howard boundary
- 1873 Howard's district boundary
- Howard's original street plan
- Existing street street plan

## **D. Utilities and Infrastructure**

**Purpose:** To upgrade Sudbrook Park's sewer and water lines, as needed, and to improve and expand the community's storm drain system, adhering as much as possible to Olmsted's directives regarding gutters.

**Background:** Sudbrook Park's infrastructure and utilities began as the result of a neighborhood design thoughtfully laid out by F. L. Olmsted, Sr. over one hundred years ago. Using large trees, extensive shrubbery and planned open spaces, Olmsted created a lush landscape that functioned, on a practical level, to absorb excess rain water. An effective, sophisticated storm drain system of terra cotta tiles and pipes handled run-off to protect large homes and tracts of property. Extensive sanitary requirements, novel for their time, were included in the early deed restrictions.

Today, original Victorian homes are surrounded by newer streets and smaller homes set amidst the community's rolling, hilly landscape. The number of homes in the neighborhood has increased from 35 in 1908 to more than 500 in 1997. Gracefully curving, uncurbed roads laid out in the 1890s intersect with several straight post-World War II cement-curbed and guttered streets, some of which lie atop a Baltimore County underground storm drain system. Utilities installed in the 1890s are no longer functional. Areas of the sewer, water and storm drain systems installed in the 1940s and 1950s are showing signs of their age. While Sudbrook Park exudes a dignified, quaint and peaceful charm, some residents suffer the occasional inconvenience of flooded yards and basements, water main breaks, electrical outages, and in rare instances, backed-up sewer lines. At times, some of these problems have resulted in significant cost to a resident when his or her insurance coverage did not apply.

Since Sudbrook Park is within a community conservation area, the following provision of the Master Plan (p. 38) is applicable:

*A significant proportion of the Capital Improvement Program should be dedicated to physical improvements in the older neighborhoods such as . . . upgrading sewer and water lines, and expanding storm drain projects. These sorts of improvements will restore pride and foster civic involvement among the communities' current residents and favorably influence prospective homebuyers."*

### **Storm Water Drainage**

The loss of over thirteen acres of trees from 1980s construction of both the rapid transit line and stations through and near Sudbrook, the addition of in-fill development and driveways, the concomitant loss of green space, and an incomplete drainage system have reduced the amount of storm water that Sudbrook Park and its environs can absorb.

Another concern of numerous residents is water seeping through walls and basement floors; this often results less from street run-off than from rising water tables after a heavy rain. Many neighbors have installed sump pumps and back-up generators. The sump pumps carry water out again, sometimes into streets and into downhill neighbors' yards, which can create additional problems.

## Water Mains

Sudbrook Park has some of the oldest water mains in the county. Over the years, breaks have occurred in various areas of the community (most recently in Spring 1997); some of these breaks resulted in damage to home and property that home owner's insurance will not cover. Given the age of the community, the County needs to be diligent in maintaining its water lines and storm drains.

## Electrical Outages

Baltimore Gas and Electric Co. (BGE) controls the electrical system for the region and has made many service improvements in Sudbrook Park in recent years. Wooded lots and large trees scattered throughout the neighborhood are essential to the character of Sudbrook Park. BGE has cooperated with the community association in not over-trimming those tree limbs encroaching on its lines. Sudbrook Park, Inc. will continue working with BGE to balance the community's desire to retain its trees and BGE's role in lessening power outages.

## Sewer Lines

Sudbrook Park also has some aging sewer lines and will work with Baltimore County to address problems as they arise. Because of the age of the community, the County needs to be diligent in maintaining the community's sewer lines.

In light of the problems and issues discussed above, and given Sudbrook Park's location in a Community Conservation Area, the following goals and actions are recommended:

**GOAL 1: To update and develop a comprehensive drainage system that will serve all existing homes and streets while maintaining the integrity of the original Olmsted design, particularly in the historic section and roads. Said drainage system should also manage run off due to water main breaks. This preventive measure may alleviate the need for replacing the water main infrastructure.**

**Action 1a:** Install an underground storm drain system where needed for those sections of Sudbrook Park which do not currently have such a system (see Appendix L).

**Action 1b:** Where cement curbs and gutters do not already exist, primarily along the original roads of Sudbrook Park, install a turf curb/gutter system appropriate to Olmsted's design that will direct street run-off to underground storm drain inlets. (The section on Roads has a detailed description of such a system).

**Action 1c:** Implement the plan for Olmsted-style landscaping on County and community owned land as this will help absorb more rainfall. This includes adding landscaped triangles or traffic circles where oversized paved intersections currently exist.

**Action 1d:** Use the County as a resource for information and possible funding by setting up a partnership between Baltimore County, Sudbrook Park, Inc., and Park homeowners that will inform homeowners of possible County options for protecting homes from water damage.

**GOAL 2: To reassess water main replacement and update the water main system when and as necessary to ensure quality water service to each household.**

**Action 2a:** Sudbrook Park, Inc. will request Baltimore County's short and long term water main replacement plans in Sudbrook Park in order to coordinate other major improvements involving neighborhood roads.

**Action 2b:** Develop a coordinated regional maintenance record that indicates accurately the history of any problems in the functioning of the water system (which is primarily maintained by Baltimore City).

**Action 2c:** Urge State elected officials, and representatives of the insurance industry, to improve coverage of property damage due to water main breaks. Inform residents who live in the small portion of the community listed as a flood plain that they qualify for flood insurance (unlike residents who live outside the flood plain).

**GOAL 3: For Sudbrook Park, Inc. to continue working with BGE as it improves service in the neighborhood, trims tree limbs near lines, and develops new approaches to equipment malfunctions brought on by weather and animals.**

**Action 3:** Sudbrook Park, Inc. will maintain a working relationship with the area BGE representative in order to communicate effectively about ongoing problems.

**GOAL 4: To be alert to any general malfunction or breakdown of the sewer line system.**

**Action 4:** Develop a county maintenance plan for the community to include regular sewer line inspections, repairs, and replacement when necessary due to the age of the system.

**GOAL 5: To protect unique and historic Sudbrook Park features (such as large or significant trees and their root systems, crowned roadways, specific road edges in the absence of curbing, and the Olmsted curvilinear road design) when repairs or replacements are made to, or in connection with, the community's utility and infrastructure systems, and to regularly maintain these systems.**

**Action 5a:** Develop a repair policy with the County to ensure proper maintenance and road repairs in harmony with Historic District standards.

**Action 5b:** Have effective coordination and input in the planning stages from appropriate Sudbrook Park, Inc. representatives to encourage County work in keeping with Sudbrook Park standards and this Plan.

**Action 5c:** Ensure that subsequent maintenance and related road repairs conform to the standards set forth in this Plan, as well as to any required Baltimore County Landmarks Preservation Commission requirements.

**Action 5d:** Make better use of the existing storm drain system by combining efforts of Baltimore County (i.e., more frequent cleaning of street and storm drains), and residents (i.e., removing leaves along curbs and gutters in front of houses).



## E. Land Use and Zoning

**Purpose:** Land use and zoning are two of the most important aspects of a community plan. Unless planned with consideration to the conservation of historic and established communities, development is likely to occur haphazardly and to the detriment of existing neighborhoods. Proper land use promotes the stability and conservation of older communities. Residents of Sudbrook Park value their unique community and want to preserve its original design characteristics and residential nature in keeping with Olmsted's intent and in light of challenges from commercial encroachment, developmental pressures from areas surrounding Sudbrook, zoning violations within the community, and transportation or other proposals which threaten the historic integrity and stability of the community.

**Background:** Sudbrook Park was a pioneer nineteenth-century residential suburb that prohibited commercial uses from its inception and has had a long and consistent history of fighting commercial encroachments. Sudbrook Park was the first community in Maryland (beginning in 1889) to have comprehensive land-use restrictions; among other things, these restrictions prohibited any trade, manufacture or business use of property in Sudbrook Park.

Olmsted himself fashioned Sudbrook's sixteen restrictions, which he deemed vitally important to the establishment of a pleasant suburban village. Not only were businesses prohibited, but only one house was allowed per acre; it had to cost a minimum amount, be in a "rural" style, and comply with certain set-backs and height limitations. Fences and hedges could not be over four feet high and lots could not be subdivided. The restrictions also applied to the construction of stables and outbuildings in Sudbrook Park; even stables had to be architecturally appropriate.

In contrast, zoning regulations for Baltimore County were not adopted until 1945. A 1940 article by Clark S. Hobbs in the *Baltimore Evening Sun* described the haphazard situation resulting from the failure to establish effective land-use controls:

[The Baltimore County Metropolitan District] sprawls helter-skelter without benefit of plan, without effective control of land use -- [a] casual, impromptu, unorganized and undirected sort of thing . . . [Y]ou can build a cow stable or a mansion pretty much where you choose, dump your garbage and trash in the nearest gully, establish an automobile graveyard where it suits your convenience . . .

(From *Master Plan History* by John McGrain, pp. 7-8).

Sudbrook Park has always been the antithesis of such development.

The major portions of Sudbrook Park are zoned D.R. 1 (density residential, one house per acre), D.R. 2 (density residential, two houses per acre) and D.R. 5.5 (density residential, 5.5 houses per acre). [See Appendix M].

For the most part, Sudbrook Park is fully developed, but there is the potential for future in-fill development on several lots large enough to permit subdivision and in undeveloped wooded areas. While there already are legislated reviews and protections for those areas of Sudbrook Park designated as a Baltimore County Historic District, it would be desirable to have design review guidelines for any future in-fill development outside and buffering the Historic District, to insure compatibility with the existing community. Action also should be taken to insure that the community's entranceway triangles and green spaces, designed as permanent open space by Olmsted, are preserved and never developed.

Just outside the boundaries of Sudbrook Park are various residential areas zoned D.R. 3.5 and D.R. 5.5; there is also an ML (manufacturing light) area adjacent to the community on its southeastern edge off Milford Mill Road, and an MLR (manufacturing light restricted) area to the northwest, along Greenwood Road. Restricting the expansion of high density and manufacturing or commercial zones around Sudbrook Park is necessary to preserve as much of a buffer as possible and to protect the community from the unsightliness, traffic, litter, noise, pollution and other pressures exerted by such development.

Based on responses to the Spring 1997 survey, residents overwhelmingly desire the community to remain residential. Over 99% of respondents support a continuation of the community association's efforts to oppose zoning violations within the community; 98.7% of respondents want to protect the residential character of Sudbrook from retail or other commercial encroachments and uses. These responses, together with Sudbrook Park's history, provide ample support for the goals and actions set forth below.

**GOAL 1a: To preserve Sudbrook Park's traditional, single-family residential community character, with the retention of its green spaces; and**

**GOAL 1b: To permanently protect Sudbrook Park's existing wooded areas and open spaces/triangles.**

*Sudbrook Park, Inc. will maintain a standing zoning committee that will actively work to carry out the above Goals through the following actions:*

**Action 1a:** Work to retain as wholly residential the zoning and character of the Sudbrook Park community.

**Action 1b:** Sudbrook Park, Inc., as part of its effort to preserve the historic fabric of the community's older homes and to prevent the introduction of commercial uses, opposes the introduction of assisted living facilities (either conversions of existing houses or new construction) in Sudbrook Park, which is a National Register and Baltimore County Historic District. Baltimore County will continue to evaluate assisted living facility projects on a case-by-case basis.

**Action 1c:** Work to prevent any higher density zoning changes within Sudbrook Park, or in the Park's buffer areas.

**Action 1d:** Work to obtain lower density zoning within and around Sudbrook Park.

**Action 1e:** Stay apprised of Baltimore County zoning ordinances and proposed changes.

**Action 1f:** Raise community awareness of applicable zoning ordinances through the community association's newsletter.

**Action 1g:** Continue to accept inquiries and complaints from Sudbrook Park residents who wish to remain anonymous and act as the primary contact or complainant to the County regarding zoning complaints and code enforcement issues/cases.

**Action 1h:** Monitor potential and reported zoning violations within or around the community.

**Action 1i:** Implement a written procedure to address zoning violations by residents ( such as untagged vehicles, parking of prohibited commercial vehicles, and prohibited business uses).

**Action 1j:** Monitor the condition of any vacant or poorly maintained houses; involve appropriate County agencies if necessary.

**Action 1k:** Work with community architects and the Sudbrook Park Landmarks Committee to develop guidelines for in-fill development both within and outside the neighborhood, with attention to the preservation of existing landscape features and, where instructive, to Olmsted's original restrictions.

**Action 1l:** Explore and propose ways to protect and preserve public and privately owned wooded areas and open spaces/triangles in Sudbrook Park, as well as residential lots large enough to permit subdivision.

**Action 1m:** Identify the locations of undeveloped parcels in the community for possible addition to the community's permanent open space system and encourage the cooperation of property owners in protecting these important natural areas.

**Action 1n:** Investigate the advantages and disadvantages of historic easements and share this information with property owners.

**Action 1o:** Address whether the MTA might wish to sell to Sudbrook Park, Inc., or protect with a permanent easement, the vacant land that it owns within and adjoining the Park in order to ensure preservation of the land as permanent open space.

**GOAL 2:** For Sudbrook Park, Inc. to maintain regular contact with the zoning office, and attend county and statewide meetings that concern zoning uses "as a matter of right" and zoning changes that might impact the community. In conjunction with this, the community association will:

**Action 2a:** Ascertain the potential effect of any proposed actions on the goal of retaining Sudbrook Park's single-family, residential character.

**Action 2b:** Propose guidelines and/or policy statements on these uses.

**Action 2c:** Encourage community activism through letters to County officials and agencies and/or seek changes in existing legislation if needed to further community goals.

**GOAL 3:** For Sudbrook Park, Inc. to participate in discussions and remain informed about proposed development and zoning changes in the communities and areas surrounding Sudbrook Park, as well as revitalization plans, town center plans, highway and conduit plans, air rights plans, zoning cycles, and the 2010 Master Plan and to study their potential impact on our community. In furtherance of this Goal, the community association will:

**Action 3a:** Oppose any new commercial development or business zoning along Greenwood Road (between Old Court Road and Sudbrook Lane).

**Action 3b:** Work to prevent a spread of commercial use, or commercial/business zoning reclassification, of properties on Sudbrook Lane, and south and west of Glenback Avenue.

**Action 3c:** Oppose additional commercial uses, or commercial/business zoning reclassification, of properties on Milford Mill Road, west of the Milford Mill Industrial Park (4400 Block of Milford Mill Road) through to Liberty Road and monitor development in buffer areas, such as Milford Mill Industrial Park.

**Action 3d:** Sudbrook Park, Inc., as part of its effort to preserve an appropriate non-intrusive buffer around the historic residential community of Sudbrook Park, opposes the construction or addition of cellular towers (or other similar such towers) within one mile of the community's borders. Baltimore County will continue to evaluate cellular tower projects on a case-by-case basis.

**Action 3e:** Maintain contact persons in other Baltimore County neighborhoods, and actively solicit their input and cooperation on issues where united action would be advantageous in maintaining our goals.

**Action 3f:** Monitor attempts by the MTA to sell and/or develop the air-rights at the Milford Mill and Old Court metro stations; oppose development deemed detrimental to the community.

## **F. Public Safety**

**Purpose:** Public safety and a feeling of security in one's home and neighborhood are crucial intangibles that both attract and retain residents. The Sudbrook Park Citizens' Patrol acts as additional eyes and ears for the local police force, and as a visible presence to discourage criminal activity within the community.

**Background:** In October 1993, several concerned residents formed the Patrol following the flagrant breaking and entering of two Sudbrook Park homes in one evening. According to police records, crime within Sudbrook Park dropped 40% in the Patrol's first year. Numbers of criminal incidents within the community have remained at almost the same level since that first year.

While Sudbrook Park is not completely free of crime and vandalism, the patrols have been a deterrent and are perceived as helpful by residents. In responding to the Spring 1997 community survey, only one person said that the patrol was not needed or wanted. Residents overwhelmingly want the Patrol to continue.

*The following are Goals and Actions of Sudbrook Park, Inc. with regard to issues of public safety:*

**GOAL 1: To educate and remind residents of ways to lessen the chance that their home will be targeted.**

**Action 1a:** Sponsor an annual talk by a police department representative; coordinate it with Sudbrook Park, Inc.

**Action 1b:** Use attachments to the Sudbrook Park, Inc. newsletter to provide information and crime-prevention tips, e.g., keep hedging and bushes low enough to preserve the field of vision to and from the house, don't let newspapers accumulate during vacations, program lights to go on during absences.

**GOAL 2: To discourage crime through lighting at individual homes rather than by increasing street lighting, which the majority of survey respondents do not want.**

**Action 2a:** Encourage residents to leave outside lights on through the night.

**Action 2b:** Advocate the installation of motion lights, when possible, to monitor activity on the side of and between houses.

**GOAL 3: To work with the Citizens on Patrol to increase the number of volunteers by actively soliciting new members on a regular basis.**

**Action 3a:** Broaden neighborhood awareness and support of the Patrol.

**Action 3b:** Consider expanding the system to use walkers and reporters who phone information to a base station.

**Action 3c:** Demonstrate the Patrol's effectiveness through the newsletter and in status updates at community meetings.

**Action 3d:** Obtain continued funding to maintain and promote the Patrol's activities through County grants when possible.

**GOAL 4: To enhance coordination with the police department.**

**Action 4a:** Designate a liaison to meet or talk at least quarterly with the Garrison precinct regarding issues of concern to Sudbrook Park.

**Action 4b:** Obtain information from the police about reported incidents in areas surrounding Sudbrook Park.

## **G. Public Schools and Education**

**Purpose:** The success of Baltimore County's commitment to revitalize older neighborhoods will be measured in large part by the success of the public schools in those areas. A critical component of this Sudbrook Park Comprehensive Plan is its emphasis on enriching, promoting, and maintaining the quality of education provided by our district primary and secondary public schools. A partnership with Baltimore County planners, educators and elected officials is necessary in order to re-establish local schools as anchors within our older neighborhoods affected by socio-economic change. Sudbrook Park, as any community, is energized by strong neighborhood schools as much as it is weakened by poor ones.

To ensure that Sudbrook Park is maintained as an economically and culturally diverse community, this Plan recommends strategies to achieve a culturally diverse school population served by professional, nurturing administrators and faculty who provide a creative and challenging academic program in a disciplined, safe environment. The success of our local schools in achieving these standards will greatly influence the future of Sudbrook Park and neighboring communities.

**Background:** The district schools for the Sudbrook Park Community are Campfield Early Childhood Learning & Development Center, Bedford Elementary, Pikesville Middle, and Milford Mill Academy. Also, residents who apply and meet the admission requirements to Sudbrook Magnet Middle School have priority over qualified "out-of-local-district" applicants because Sudbrook had been our district school before being converted to a magnet school.

Our kindergartens are located at the Campfield Center. Its impressive and extensive "special needs" facilities for children two to four years of age are also available to our residents.

The composite MSPAP score at Bedford rose 13.8 points from 1994 to 1996. It was 4.1 points lower than the Baltimore County average in 1996. In 1997, an increase of 11.1 points brought Bedford's composite score to 51.8 — 4 points above the County's average. Bedford now ranks 9th among the 18 Northwest Area elementary schools with respect to MSPAP composite scores. In spite of the improvement in scores, improved faculty stability, staff development and consistently excellent leadership, many parents in Sudbrook Park and other Bedford district communities do not perceive Bedford Elementary as a quality school. (Responses to the community survey and input obtained through a series of parent-to-parent meetings indicate that a racial composition not reflecting the community, test scores and perceptions of problems with discipline are deterrents to enrollment of many Sudbrook Park children at Bedford.)

Despite the improvement noted above, anything short of ambitious initiatives endangers the progress evidenced in Bedford's scores and in the school itself. According to a listing in the Baltimore Sun (Dec. 22, 1997, p. 4B), Bedford's third grade MSPAP scores ranked it 64<sup>th</sup> in Reading and 40<sup>th</sup> in Math out of the 98 Baltimore County Elementary Schools. The fifth grade ranked 48<sup>th</sup> in Reading and 34<sup>th</sup> in Math out of 98.

The parent-to-parent meetings and the 161 returned surveys (a 32% return) confirm the following relevant information:

1. The area schools are a top priority concern of residents in Sudbrook Park.

*Survey Question 5. The quality of our local public schools is essential for the maintenance of Sudbrook Park as a stable, culturally diverse community. Should measures to improve Camp field (preschool/kindergarten), Bedford Elementary, Sudbrook Magnet Middle, Pikesville Middle and Milford Academy (U.S.) be included in the plan?*

Yes: 124      No: 4      N/A: 33 (none have school-aged children)

2. There is an increase in private school enrollment at the elementary level. Of 24 elementary school-aged children mentioned in survey responses, only 8 (one-third) attend public schools, and only five of the eight attend Bedford. (Table 1 [also includes public/private school attendance at the middle and high school level])

**Table 1. Public/Private Schools attended by Sudbrook Students**  
(whose parents responded to survey)

	Total	Private	Home Schooled	Public
Elementary	24	15	1	8: Bedford-5, Milbrook-1, Ft. Garrison-2
Middle	13	6		7: Sudbrook Middle-5, WildeLake-1, Pikesville-1
High School	12	1		11: Milford Academy-5, Pikesville Sr.-5, Carver-1
Totals	49	22	1	26

3. Increased private school enrollment can be anticipated at the kindergarten/1st grade level. Survey responses indicate that, of 30 Caucasian pre-schoolers, parents of 11 (37%) plan to send them to private/parochial schools, one will be home-schooled, 10 (33%) are undecided, and 8 (27%) are planning on public schools — but only 3 of these 8 intend to enroll at Bedford. Some respondents commented on the possibility of moving to another school district before school age is reached.

*Survey Question 7. (Underlined numbers indicate respondent totals.) Please indicate the age(s) of any preschoolers in your household 30. Are they attending preschool now? Yes 16 No 14. If so, where? \_\_\_\_\_ Do you plan to enroll them in a Private 11 or Public 8 school? 10 Undecided. 1 Home-Schooled.*



4. From the survey and parent contacts we know there are at least sixty-one Caucasian pre-schoolers currently living in Sudbrook Park. If these children were to enroll at Bedford Elementary, the school population would more accurately reflect the demographics of the surrounding community there in the next few years (Table II).

Table II. Age Distribution of Identified Caucasian Pre-Schoolers in Sudbrook Park

0-12 mos	1 to 2 yrs	2 to 3 yrs	3 to 4 yrs	4 to 5 yrs	5 to 6 yrs	6 yrs	Age not known	Total
8	12	8	11	6	8	1	7	61

If Sudbrook Park, a proclaimed Baltimore County Conservation Area, is to maintain its viability as a stable, culturally and economically diverse community, then its citizens and those of the other Bedford Elementary school district communities, administrators in the public school system, school board members and appropriate elected officials must all cooperate to find and act upon solutions to the current situation. This will not only benefit our community, but will enhance quality education and future stability in Bedford Elementary and its district, rather than have it decay as families with school aged children move away from the public schools.

Both middle schools (Sudbrook Magnet Middle and Pikesville Middle) that serve our community have above-County average and rising MSPAP scores. They are culturally diverse and appear to have wide community acceptance as quality schools. In 1997 scores Sudbrook Magnet Middle ranks #1 in the County in reading and #4 in math. Pikesville Middle ranks #2 in reading and #5 in math. The singular, but none the less significant, concern is an absence of continuity in some magnet programs at the high school level.

The Milford Mill Academy Guidance Department advises that about 70% of the June '97 graduating class went on to either two or four-year colleges, about 12-15% to technical/specialty training programs, and about 5% to the military. Those who did not meet graduation requirements either went to summer school or returned this fall to fulfill them. Last year's dropout rate for the entire school was very low, about 2%. The current enrollment of 1,250 students is about 100 less than expected, placing it at a comfortable building capacity. The International Baccalaureate Program with its prestigious and academically demanding world-wide curriculum is "alive and well" with about 35 seniors participating this year as compared to 18 last year.

Milford Mill Academy has also been a source of serious concern. The school has had several publicized negative occurrences in the past few years, leading to perceptions of an unsafe environment. The school, with less than a 5% Caucasian population, does not accurately reflect the demographics of the surrounding community despite offering specialized programs. Several Sudbrook Park families feel the need to relocate or enroll their offspring in a private school as they attain high school age. Consequently, the Education Committee corresponded with the County Board of Education members (see Appendix N) and other relevant administrators urging

redistricting of Sudbrook Park's public high school students (currently 44 in number) to Pikesville Senior High School, if there would not be a change in existing academics and demographics.

In the hope that the Baltimore County government, the Baltimore County Public School System, and the residents of Sudbrook Park can work together to conserve our and other area communities, the following are Sudbrook Park, Inc's goals and actions for maintaining, enriching and promoting the quality of education provided by our district schools.:

***Sudbrook Park, Inc. will pursue the following Goals and Actions:***

**Bedford Elementary/Campfield Center**

**GOAL 1: To improve community perceptions of both schools by exposing area residents to their academic commitment and their innovative cultural and extra-curricular programs.**

**Action 1a:** Use Sudbrook Park's newsletter, community bulletin board, and the *Owings Mills Times* to inform the public of special school programs, MSPAP scores, awards, grants, etc.

**Action 1b:** Invite PTA, area principals, and other administrators to speak to Sudbrook Park parents.

**Action 1c:** Continue parent-to-parent meetings.

**Action 1d:** Market before- and after-school daycare opportunities in the community and its schools.

**Action 1e:** Invite parents of pre-schoolers to schools during American Education Week.

**Action 1f:** Develop informational brochure for realtors.

**Action 1g:** Seek cooperation and communication with other feeder communities.

**GOAL 2: To promote the development of a magnet and/or enrichment program at Bedford. This may encourage enrollment of residents of Sudbrook Park and neighboring communities and also provide stimulation and enrichment to children with specific interests or talents. The new program should be in harmony with one of the programs at Sudbrook Magnet Middle School, e.g., instrumental music, foreign language, computer science.**

**Action 2a:** Seek support of the PTA, Principal, NW Area Superintendent, Superintendent of Schools, Board of Education.

**Action 2b:** Seek financial support from the Board of Education and County government.

**Action 2c:** Seek cooperation of other communities within Bedford district.

**Action 2d:** Investigate magnet program possibilities that tie into Sudbrook Magnet's programs.

**GOAL 3: To lobby for the addition of a Gifted and Talented Specialist at Bedford who would be a resource for enrichment of the curriculum and enhancement of the current Gifted and Talented program.**

**Action 3a:** Seek support of the PTA, Principal, NW Area Superintendent, Superintendent of Schools, Board of Education.

**Action 3b:** Seek funding for the position.

**Action 3c:** Seek cooperation of other communities within Bedford District.

**GOAL 4: To provide community support for Bedford/Campfield.**

**Action 4a:** Encourage residents to support school fund-raising projects and activities sponsored by the PTA(s), by publicity, individual contributions, etc.

**Action 4b:** Lobby elected officials and Board of Education.

**Action 4c:** Seek out businesses/senior organizations to form partnerships with schools.

**Action 4d:** Participate in donation programs (Giant and Metro receipts, etc.)

**Action 4e:** Encourage volunteerism.

**Action 4f:** Establish a liaison with other Bedford/Campfield communities to work on common challenges.

**GOAL 5: To urge the School Board to consider additional strategies for achieving racial balance in all schools in the community.**

**Action 5a:** Work with the Board of Education to look at various options including the creation of magnet or special programs and/or redistricting for increasing community enrollment in the local districted schools.

**Action 5b:** Communicate with other districts as to their strategies in dealing with similar problems.

**Sudbrook Magnet Middle/Pikesville Middle**

**GOAL 6: To educate the community about choices between district schools and magnet schools and support successful school programs.**

**Action 6a:** Keep the community informed about the quality programs at Pikesville Middle School, our district school.

**Action 6b:** Maintain a high level of community support for successful programs at Sudbrook Magnet Middle.

**Action 6c:** Support the continuation of existing high school magnet programs and the development of others to provide continuity for Sudbrook Magnet Middle graduates.

**Milford Mill Academy/Baltimore County Magnet High Schools**

**GOAL 7: To educate the community about the choices between Milford Mill Academy (our district school) and magnet schools.**

**Action 7a:** Keep the community informed about Milford Mill Academy's comprehensive programs, as well as the International Baccalaureate program, and the many career development programs.

**Action 7b:** Establish a line of communication between Sudbrook Park and the administration at Milford Mill Academy.

**GOAL 8: To investigate strategies to attract a diverse student population.**

**Action 8a:** Maintain a high level of support for successful programs at Milford Mill Academy.

**Action 8b:** Contact county educators and elected officials for assistance.

## **H. Sudbrook Stream Valley Park**

**Purpose:** The preservation of open space for gatherings was one of the design concepts Olmsted used in his suburban villages to create a "sense of community" among residents. To that end, his plan for Sudbrook designated an 8.5 acre area of permanent open space, called "Cliveden Green," in addition to landscaped triangles placed about the community, to foster the "harmonious association" of residents. Although the Cliveden Green area was developed in the 1940s and was not preserved as open space, Sudbrook Park has always had a designated open space area for gathering and recreation. Since 1971, that area has been the Sudbrook Stream Valley Park (SSVP) near McHenry Road (see Appendix O).

SSVP provides critical greenspace for the community, and since much of the area is in a flood plain, it is unsuitable for development. Preserving, retaining and enhancing this permanent open space is important to the residents of Sudbrook Park.

**Background:** Cliveden Green, the permanent open space which was to have been situated between Upland and Milford Mill Roads, bordered on the east by what is now the 600 block of Cliveden Road and on the west by what was to be an extension of Cliveden Road West to Milford, was built-up in the 1940s. About 1948, several residents deeded land north of the S-curve at Greenwood Road to the community association for a community park, playground and recreational area. The area also became a place for celebrating long-established holiday traditions such as the Fourth of July and other seasonal celebrations.

The community lost its Greenwood Road park and playground in the early 1970s when the State purchased land in that area to route the Northwest Expressway through Sudbrook Park. In response, Baltimore County Parks and Recreation created a new park for the community: Sudbrook Stream Valley Park, off McHenry Road, was equipped with playground equipment and formally dedicated in 1971. In addition to being the gathering site for community celebrations and traditions (such as the July Fourth and Halloween parades, a Spring Social with games, and a fall "Music in the Park" event), the park hosts daily visits by neighborhood children and parents who utilize the park and playground equipment (which dates back to the park's creation).

In the recent survey of residents, 87.9% of respondents acknowledged that they use the SSVP. Desired areas of improvement, in priority order, were identified as: drainage, playground equipment, establishment of a bike trail that connects with local bike trails in the area, landscaping, creation of a tennis court area, and establishing a picnic shelter area. While eliminating drainage problems is the top priority of respondents, it is not a viable goal because the SSVP is located within a flood plain. It may be possible, however, to improve drainage through landscaping and other measures. Although a number of survey respondents indicated interest in establishing a bike trail, that issue has been controversial in the past among residents and will require additional study.

Recognizing that the Sudbrook Stream Valley Park has limitless possibilities for enhancement, that the park can easily be divided into different activity areas, and that the community is dedicated to preserving it as a permanent open space and a focal point for community and family gatherings, the community proposes to work cooperatively with the Baltimore County Department of Recreation and Parks, the Department of Environmental Protection and Resource Management, and outside consultants to achieve the following:

**GOAL 1: To develop a plan for acquisition of funding to realize community vision for improvements to the Sudbrook Stream Valley Park (SSVP).**

**Action 1a:** Identify funding resources through public and private sectors, such as the Community Assisted Enhancement of Facilities program offered by the Department of Recreation and Parks.

**Action 1b:** Evaluate proposed improvements, giving proper consideration to budget and available space.

**GOAL 2: To acquire new recreational playground equipment.**

**Action 2a:** Re-design the park's recreational environment with proper consideration to age appropriateness, accessibility, and safety.

**Action 2b:** Address aesthetics and the design and use of the overall space.

**Action 2c:** Identify vendors and equipment types which best meet the needs of the community.

**GOAL 3: To design for the SSVP a comprehensive landscape plan in accordance with Olmstedian principles.**

**Action 3a:** Identify recognized Olmsted experts and landscape and design professionals to assist in creating a plan.

**Action 3b:** Investigate landscaping concepts which may alleviate some of the drainage problems.

**Action 3c:** Design recreational space with attention to its aesthetic integration in the overall landscape.

**GOAL 4: To convert existing basketball courts into tennis courts.**

**Action 4:** Work with County Recreation and Parks to evaluate the feasibility of converting existing basketball courts into tennis courts.

**GOAL 5: To investigate the possibility of obtaining a covered shelter for recreational use.**

**Action 5a:** Consult with County officials (DEPRM and Recreation and Parks) regarding building specifics, environmental impact, ownership and liability issues.

**Action 5b:** Obtain a design for a simple, functional and architecturally appropriate structure to meet community needs.

*Sudbrook Park, Inc. will pursue the following Goal and related actions:*

**GOAL 6: To revisit the idea of building a bicycle trail which may or may not connect with adjacent neighborhoods.**

**Action 6a:** Hold a community meeting to obtain broader input from Sudbrook Park residents and more fully air concerns voiced earlier about implementing a bike trail.

**Action 6b:** Meet with neighboring communities to determine if there is area-wide support for creating a bicycle path.

**Action 6c:** If the level of community support for a bike path is high, work with residents, the County and neighboring communities to determine the best route and address issues such as environmental impact, acceptable surfacing materials, width, design, lighting and safety.

**Action 6d:** Establish parameters for an on-going maintenance plan with the County and other affected communities.

### III. CONCLUSION

This Plan sets forth both short and long term goals for the community of Sudbrook Park, along with specific actions intended to implement these goals. Although planning is never a *fait accompli* and new developments, additional issues or further study may result in modifications or additions to these goals and actions in the future, the course mapped out by this Plan represents a consensus of the attributes that Sudbrook Park residents have said they want to retain and the problems that they have identified.

The community will need to work actively with the County, State and other preservation organizations such as the National Trust for Historic Preservation and Preservation Maryland to ensure that maximum effort is made to:

- Preserve the historic aspects of the neighborhood (bridge, roadways, triangles and trees -- the Olmsted design).
- Restore the Olmsted design where feasible.
- Protect the residential character of the community by prohibiting commercial encroachment.
- Improve the perception of the quality of schools.
- Decrease the number of speeding vehicles traveling through Sudbrook Park.
- Minimize further green space erosion.
- Insure adequate drainage and utility systems.
- Enhance the Sudbrook Stream Valley Park.
- Monitor proposed land use changes to protect the community's historic character.

To achieve the objectives described in this Plan, Sudbrook Park, Inc. will need to communicate the community's design objectives clearly to the County and retain the support of a majority of residents. While it is not expected that there will be 100% agreement on every issue, a majority of Sudbrook Park residents have traditionally supported -- and continue to support -- efforts to preserve the historic aspects of the community and the special quality of life that these attributes nurture.

The preparation of this Plan over a period of twenty months has provided an opportunity for residents to explore and consider the issues the community does and will face in the future. With the assistance of Baltimore County, this Plan -- the end result of an intensive community effort -- will be the new blueprint to help guide Sudbrook Park into the twenty-first century.

In conclusion, two written comments submitted by residents at the May 20, 1998 community input meeting, reproduced below, represent the consensus which a majority of residents have communicated to Sudbrook Park, Inc. regarding this Plan:

*Great Plan! Sudbrook Park is a National Historic Landmark District and deserves special attention toward resolving community problems with a nod to Olmsted's design. The County should support the recommendations made by the community in this Plan by providing creative solutions to the issues outlined. If implemented and supported, Sudbrook Park can be not only a local asset, but a boon to tourism and quality of life within Baltimore County. It is an answer to the west side of Pikesville. Help us by doing for our infrastructure what most of us have done for our own properties . . . Restore It!*

*I am wholeheartedly in favor of the Comprehensive Plan -- it represents an enormous effort on a large number of people's parts and is a great first step in helping Sudbrook Park move gracefully into the millennium.*



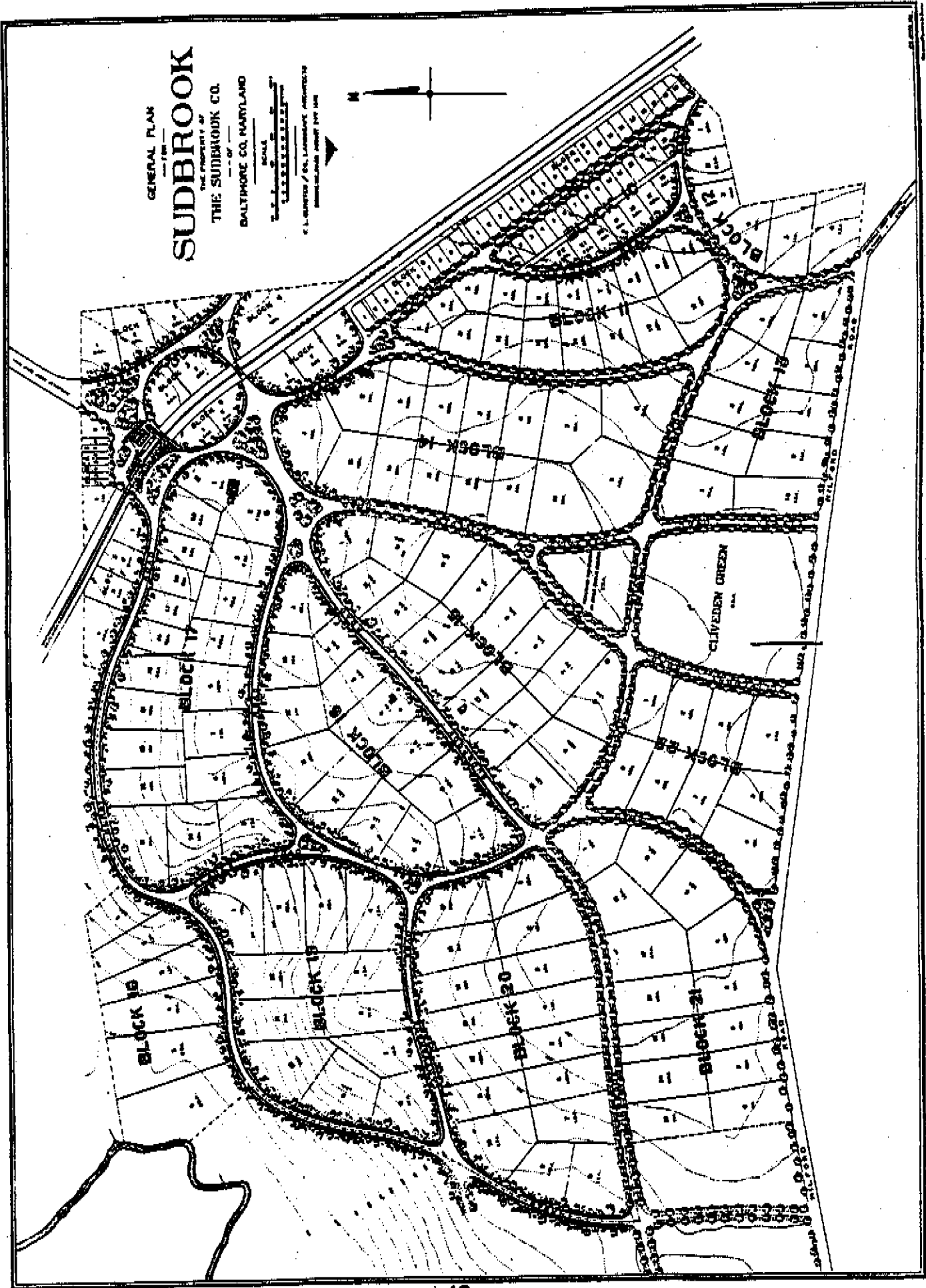
## Sudbrook Park Community Plan Implementation Guide Recommendations

Goal/Objectives	Participants	Action Steps	Resources
<b>1. Promote Sudbrook Park's History</b>	<ul style="list-style-type: none"> <li>• Sudbrook Park Inc.</li> </ul>	<ul style="list-style-type: none"> <li>• Schedule periodic events to present Sudbrook's History</li> <li>• Correct Historic Markers</li> <li>• Explore expanding the district boundaries</li> </ul>	<ul style="list-style-type: none"> <li>• <u>Olmsted's Sudbrook. The Making of a Community</u></li> <li>• The Maryland Historic Trust</li> <li>• The Baltimore County Landmarks Preservation Commission</li> <li>• DEPRM</li> <li>• Save Our Streams</li> <li>• Neighborhood Design Center</li> </ul>
<b>2. Restore Sudbrook Park's bridges, roads, and landscaping to be consistent with the original Olmsted Design</b>	<ul style="list-style-type: none"> <li>• Sudbrook Park Inc.</li> <li>• The Office of Community Conservation</li> <li>• The Department Of Public Works</li> <li>• Neighborhood Design Center</li> </ul>	<ul style="list-style-type: none"> <li>• Maintain roads, bridges and landscaped triangles as proposed in the original Olmsted design principles</li> <li>• Determine extent of restoration to be pursued</li> </ul>	<ul style="list-style-type: none"> <li>• The Maryland Historic Trust</li> <li>• The Landmarks Preservation Commission</li> <li>• <u>Olmsted's Sudbrook. The Making of a Community</u></li> </ul>
<b>3. Implement measures to discourage speeding</b>	<ul style="list-style-type: none"> <li>• Sudbrook Park Inc.</li> <li>• The Bureau of Traffic Engineering and Transportation Planning</li> </ul>	<ul style="list-style-type: none"> <li>• Determine the feasibility and impact of additional traffic calming</li> </ul>	<ul style="list-style-type: none"> <li>• Baltimore County Police-Western District Traffic Division</li> </ul>
<b>4. Improve Infrastructure and Utilities while preserving Olmsted design features</b>	<ul style="list-style-type: none"> <li>• Sudbrook Park Inc.</li> <li>• The Department of Public Works</li> <li>• The Office of Community Conservation</li> </ul>	<ul style="list-style-type: none"> <li>• Identify infrastructure needs</li> <li>• Maintain Olmsted design features whenever possible</li> </ul>	<ul style="list-style-type: none"> <li>• The Department of Public Works</li> <li>• Neighborhood Design Center</li> </ul>

<b>Goal/Objectives</b>	<b>Participants</b>	<b>Action Steps</b>	<b>Resources</b>
5. <b>Maintain the zoning and Master Plan designated land use</b>	<ul style="list-style-type: none"> <li>• Sudbrook Park Inc.</li> <li>• The Office of Planning</li> <li>• The Office of Community Conservation</li> </ul>	<ul style="list-style-type: none"> <li>• Discourage zoning changes</li> <li>• Prohibit uses which increase traffic</li> </ul>	<ul style="list-style-type: none"> <li>• The County Council</li> <li>• The Board of Appeals</li> <li>• The Planning Board</li> </ul>
6. <b>Maintain a safe community</b>	<ul style="list-style-type: none"> <li>• Sudbrook Park Inc.</li> <li>• Citizens on Patrol</li> </ul>	<ul style="list-style-type: none"> <li>• Increase citizens participation</li> <li>• Attend monthly Police Community Relation Meetings</li> </ul>	<ul style="list-style-type: none"> <li>• The Baltimore County Police Department</li> </ul>
7. <b>Promote quality Schools</b>	<ul style="list-style-type: none"> <li>• Sudbrook Park Inc.</li> <li>• The area PTA's</li> </ul>	<ul style="list-style-type: none"> <li>• Invite PTA, area principals and other administrators to speak to Sudbrook Park parents about quality programs at area schools</li> <li>• Encourage a high level of support for area schools</li> <li>• Urge the School Board to consider additional strategies to improve racial balance</li> </ul>	<ul style="list-style-type: none"> <li>• The Board of Education and the Northwest Area Superintendent</li> <li>• The PTA</li> <li>• The County Council</li> <li>• The State Legislators</li> </ul>
8. <b>Enhance Sudbrook Stream Valley Park</b>	<ul style="list-style-type: none"> <li>• Sudbrook Park Inc.</li> <li>• The Pikesville Recreation Council</li> </ul>	<ul style="list-style-type: none"> <li>• Enhance existing Park by acquiring updated recreational equipment, landscaping</li> <li>• Create a more picturesque Olmstedian landscape</li> </ul>	<ul style="list-style-type: none"> <li>• The Department of Recreation and Parks</li> <li>• Department of Public Works Storm Water Management Division</li> <li>• The Department of Environmental Protection and Resource Management</li> </ul>

## **APPENDICES**

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GENERAL PLAN  
FOR  
**SUDBROOK**  
THE PROPERTY OF  
THE SUDBROOK CO.  
OF  
BALTIMORE CO. MARYLAND

SCALE  
1" = 100'  
L. SUDBROOK & CO. ARCHITECTS  
BALTIMORE, MARYLAND 1914



## Sudbrook - A Brief History

by Melanic Anson (1996)

Sudbrook was designed in 1889 by Frederick Law Olmsted, Sr. (1822-1903), the founder of landscape architecture in America. Olmsted, co-designer of New York City's Central Park, was a visionary who foresaw the trend toward suburban living before it became an established phenomenon. In 1869, he completed a design for Riverside near Chicago, his first suburban village, noting that "no great city can long exist without great suburbs." Riverside, Sudbrook, and Druid Hills (Atlanta) are the only surviving residential designs of Olmsted, Sr. Each was planned to encourage the "harmonious association" of residents and to provide a tranquil "respite for the spirit" -- in contrast to crowded and controlling conditions in cities.

Before becoming a suburban experiment, "Sudbrook" was the estate of James Howard McHenry. McHenry was a gentleman farmer descended from a distinguished Maryland family, who had large land holdings in the Pikeville area in the 1870s. In 1876, McHenry contacted Olmsted about designing a suburban village on his Sudbrook estate, but no plan materialized. Following McHenry's death in 1888, a group of investors from Boston and Philadelphia incorporated as the Sudbrook Company. The Company worked with Olmsted, who was assisted by his adopted son and partner, John Charles Olmsted, to design a suburban community on 204-acres purchased from the McHenry estate.

Olmsted's design for Sudbrook, exceptional for its time, remains a work of art. In an age when streets were arrow-straight, Olmsted's roads were all curvilinear. So unusual were curving roads that the Sudbrook Company initially could not find a surveyor who could lay them out on the ground. Olmsted created a distinct approach and entranceway for Sudbrook and suggested sixteen deed restrictions that governed lot size and setbacks, excluded commercial activities, required acceptable sanitation practices, and limited cows and horses (pigs were prohibited). Although the majority of lots were about an acre, Olmsted also included smaller lots, a practice not common at the time.

Olmsted was particular about trees and vegetation and their placement. He incorporated open green spaces throughout his design, and intended that such areas be used by the community for holiday fetes and informal gatherings. From its earliest days, Sudbrook has used its green spaces for community gatherings and still has parades and festivities to celebrate various holidays.

Olmsted designed Sudbrook as a year-round suburban village and the Sudbrook Company constructed nine "cottages" and an inn to entice potential purchasers to buy immediately and to stimulate further sales. Baltimoreans, however, were slow to accept the idea of residing permanently eight miles from the city, a significant distance before the automobile. From its opening in 1890, Sudbrook (renamed "Sudbrook Park" by its developers) attracted prominent Baltimoreans eager to rent for "the season" (May through October).

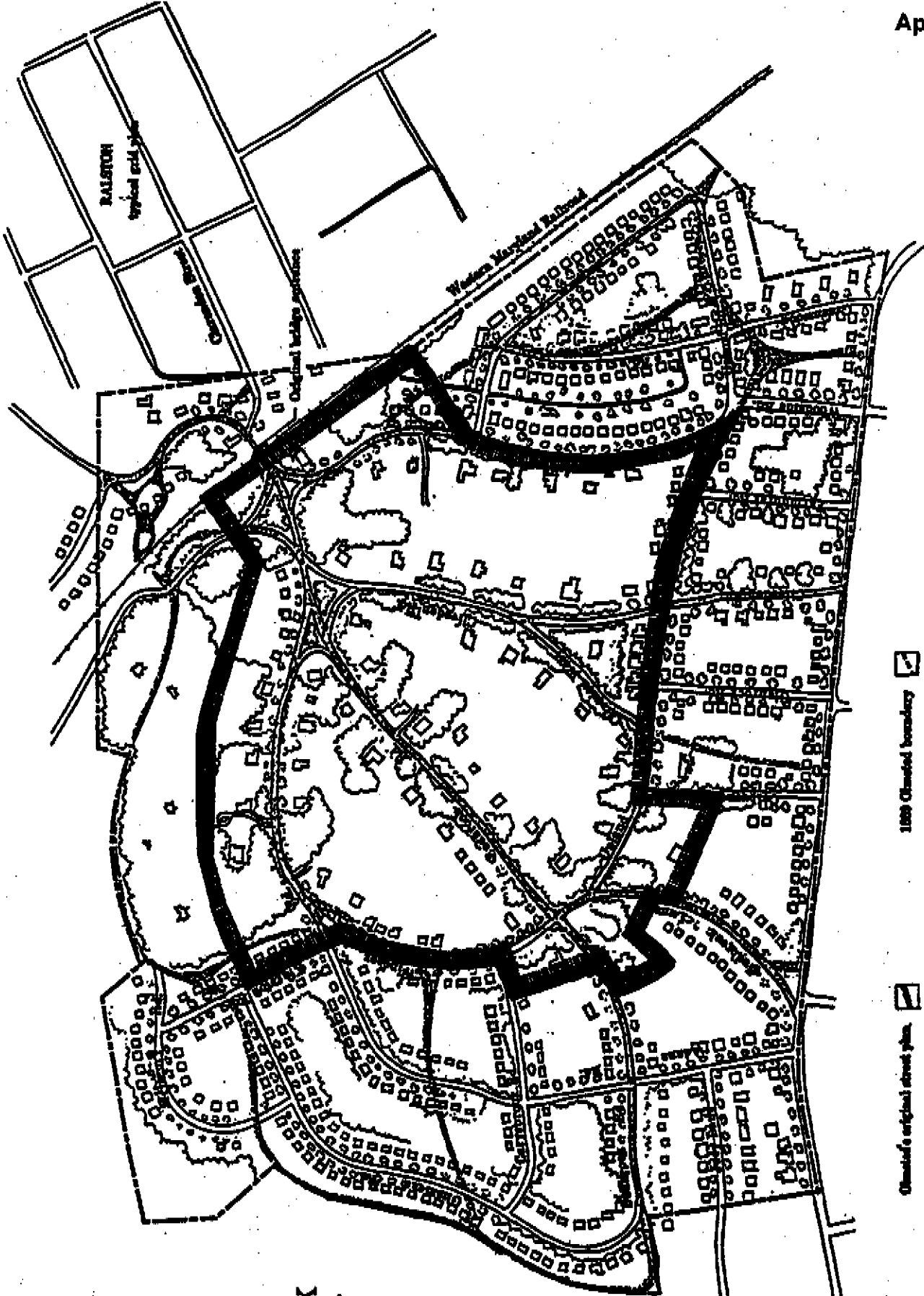
and to stay at the inn, which became the social center of the community. While Sudbrook had a few year-round renters from the beginning and a growing population of permanent residents each year, sales never materialized to the extent anticipated. The Sudbrook Company's inability to sell a substantial number of lots, combined with the popularity of the seasonal hotel -- whose occupants more than doubled the size of the community -- led to Sudbrook Park's early reputation as a "summer community."

By 1898, suburban developments closer to the city, and on electric trolley lines that ran more frequently, began to grow rapidly. One of these suburbs was Roland Park, designed in part by Olmsted's son, Frederick Law Olmsted, Jr. Himperted by Sudbrook's distance from the city and other factors, the Sudbrook Company went out of business around 1910, having overseen the construction of about thirty-five houses. Ten to fifteen more houses had been built by the time the hotel burned in 1926. Further development ceased with the Great Depression of 1929.






From 1939 to 1954, Sudbrook's remaining land was subdivided and hundreds of neo-colonial and cape cod style brick houses were built around the early cottages. Although the new development altered aspects of Olmsted's design, the artful skill of Olmsted's planning principles was powerful enough to mold the diverse parts into a unified whole.

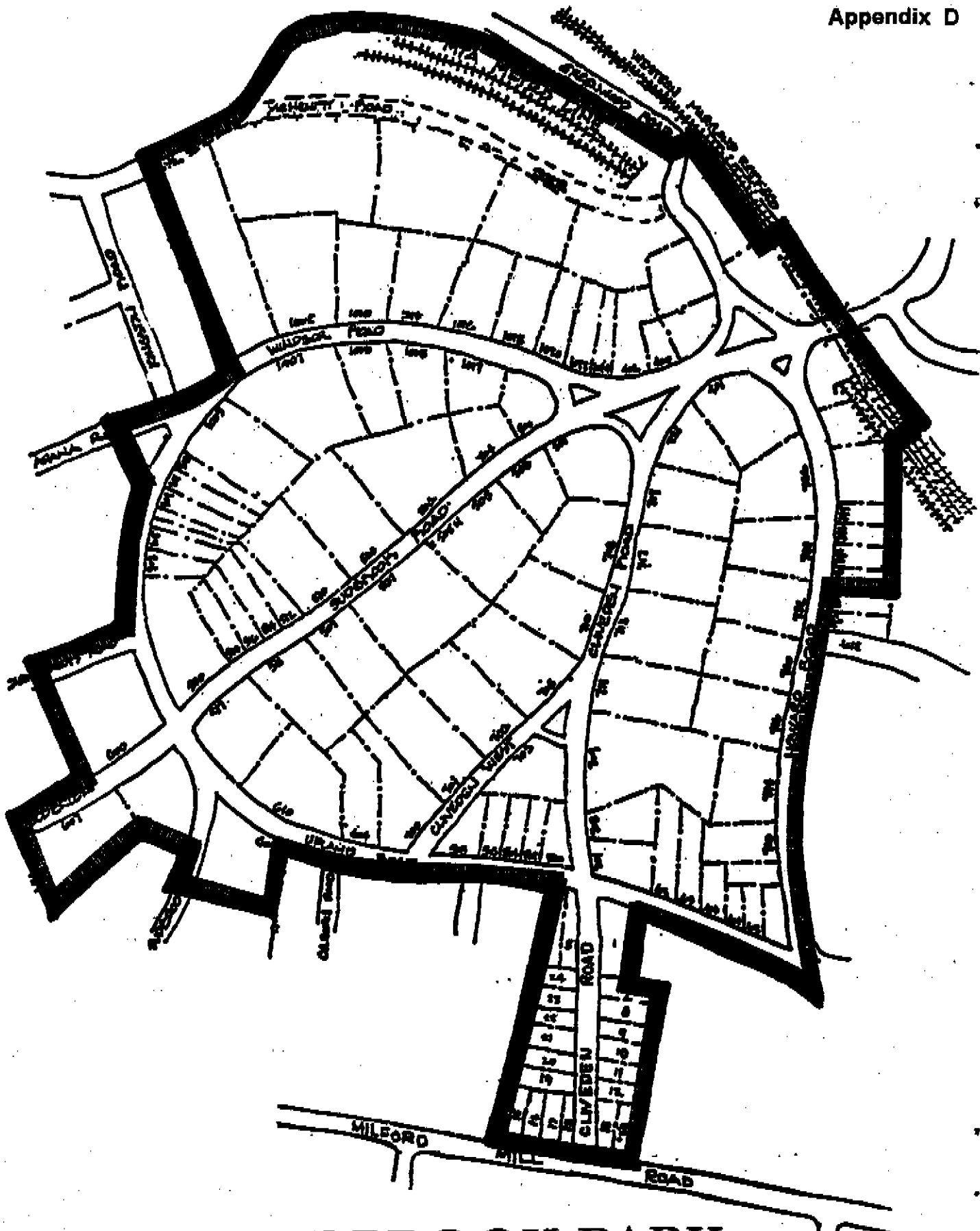
In the 1960's, with no regard for Sudbrook's Olmsted-lineage and national historic significance, the State opted to build a six-lane expressway through the community. After a portion of Sudbrook Park was entered on the National Register of Historic Sites and Places in 1973, the State agreed to delete the section of the highway through Sudbrook. In the late 1970's and early 1980's, the community again mobilized to minimize the adverse impact of a planned rapid transit line through the community. Compromises between Sudbrook and the Mass Transit Administration permitted Sudbrook to retain its narrow gateway bridge (a key element of Olmsted's design), preserve its original street layout, save numerous trees that otherwise would have been felled and negotiate a replanting plan. In 1993, a portion of Sudbrook Park became a recognized Baltimore County Historic District; an addition to this area was made in 1995.

Sudbrook has weathered many changes since it was originally planned as an innovative "suburban village" by America's first and foremost landscape architect. Today, the towering oaks that once formed a massive green umbrella near the entranceway bridge are gone, as is the hotel from another era. What remains is a tribute to Olmsted's vision -- a design that is more than the sum of its parts, more than just artfully designed curvilinear roads, majestic trees, open green spaces, turn-of-the-century and World War II era homes. Thanks to Olmsted's genius and the well-preserved elements of his design, Sudbrook Park remains a cohesive community in the true sense of the word and a "respite for the spirit," as important now as it was 107 years ago.



General Plan of  
**SUDBROOK**  
 Scale: 1" = 200'  
 ↑

-  1888 Oldwood boundary
-  1873 Historic District boundary
-  National Register District
-  Oldwood's original street plan
-  Existing street street plan



# SUDBROOK PARK

Baltimore County Landmark District Map



*View into Sudbrook from entranceway bridge, 1945*



*View from bridge into Sudbrook, 1996*





*View of bridge from within Sudbrook, 1960*

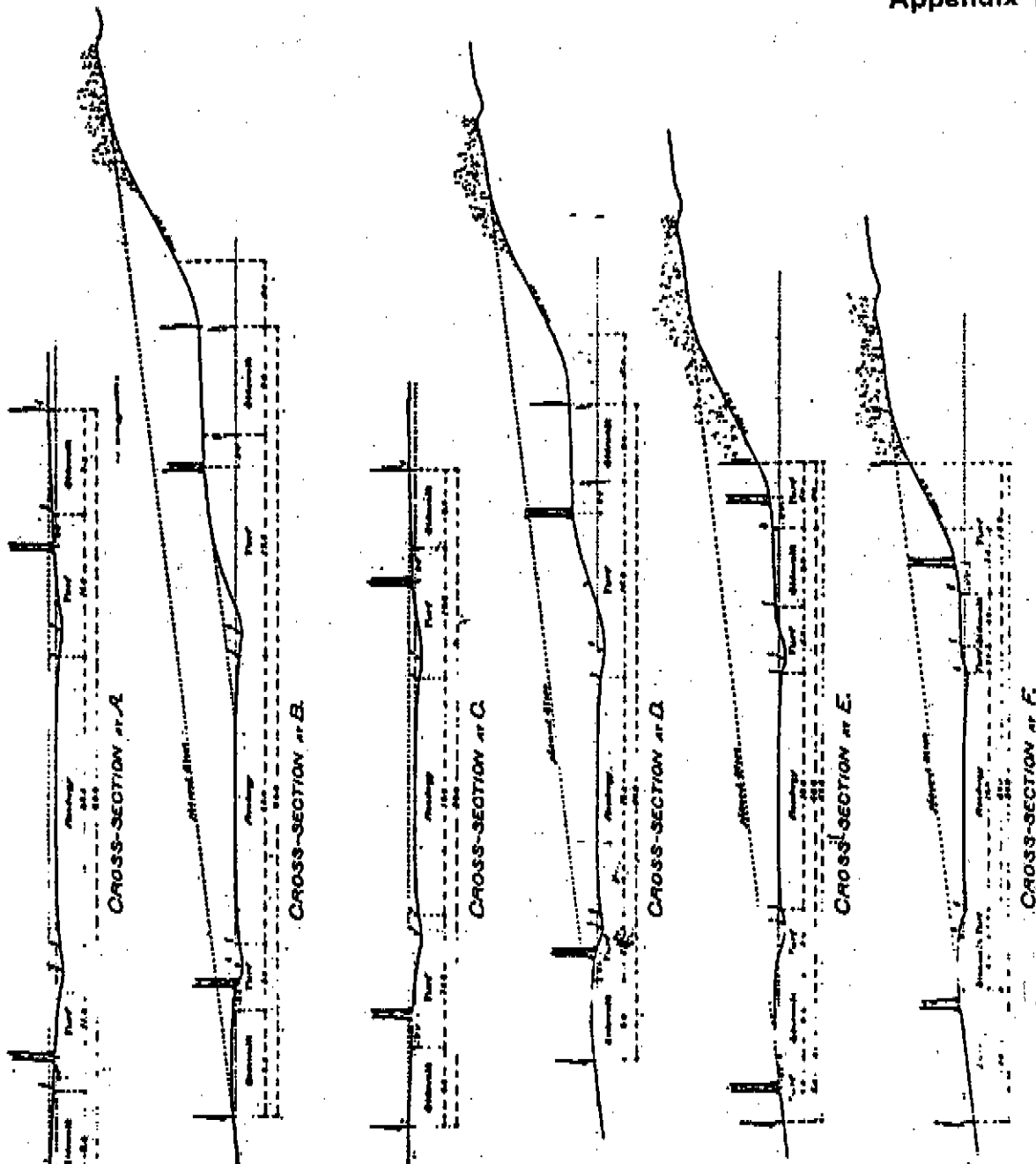


*View of bridge from within Sudbrook, 1996*

THE SUBBROOK LAND COMPANY OF BALTIMORE CO.

CROSS-SECTIONS FOR ROADS

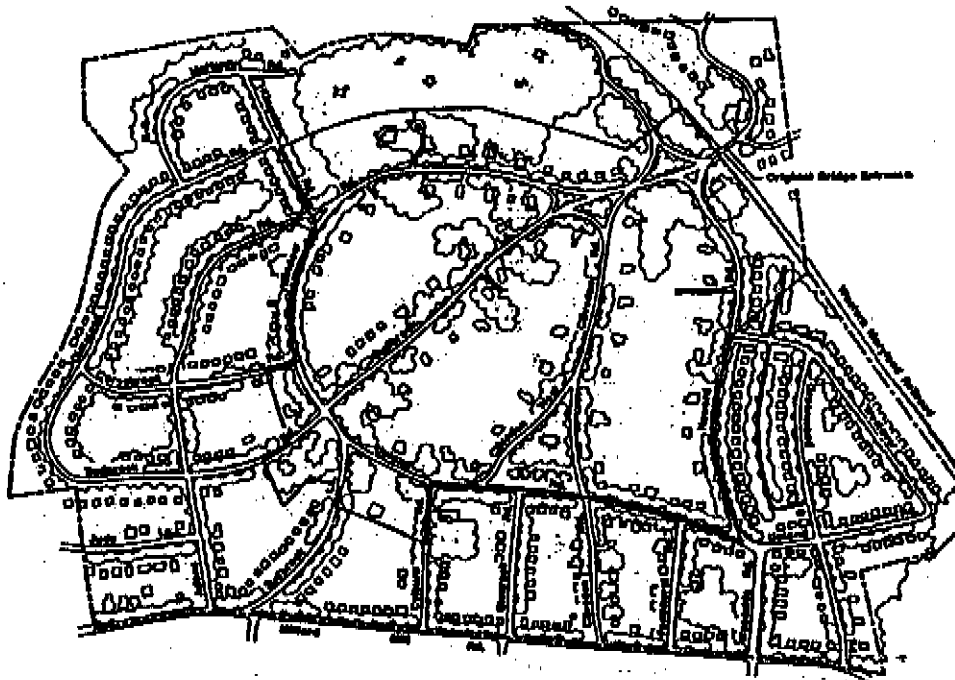
SCALE 3 FEET TO AN INCH  
 25' UNBLENDED LIMESTONE CONCRETING MASS



Explanations

- A. - Shows the road surface, concrete base, and subgrade. The concrete base is 6 inches thick and is made of 25' unblended limestone concreted mass. The subgrade is 6 inches thick and is made of the same material.
- B. - Shows the road surface, concrete base, and subgrade. The concrete base is 6 inches thick and is made of 25' unblended limestone concreted mass. The subgrade is 6 inches thick and is made of the same material.
- C. - Shows the road surface, concrete base, and subgrade. The concrete base is 6 inches thick and is made of 25' unblended limestone concreted mass. The subgrade is 6 inches thick and is made of the same material.
- D. - Shows the road surface, concrete base, and subgrade. The concrete base is 6 inches thick and is made of 25' unblended limestone concreted mass. The subgrade is 6 inches thick and is made of the same material.
- E. - Shows the road surface, concrete base, and subgrade. The concrete base is 6 inches thick and is made of 25' unblended limestone concreted mass. The subgrade is 6 inches thick and is made of the same material.
- F. - Shows the road surface, concrete base, and subgrade. The concrete base is 6 inches thick and is made of 25' unblended limestone concreted mass. The subgrade is 6 inches thick and is made of the same material.

# SUDBROOK PARK



- 1873 Historic Model Boundary
- 1939 Planned Boundary

From : Second District Scenic  
Routes Brochure,  
Baltimore County  
Office of Planning

To visit a village from a later era, drive through the community of Sudbrook Park, just south of Pikesville. Sudbrook Park, a National Register Historic District, has the distinction of being the only Maryland community planned by America's first landscape architect, Frederick Law Olmsted. One hallmark of an Olmsted design is a distinct entranceway, or "gateway," that defines a community. As the entrance to Sudbrook Park, Olmsted planned the narrow bridge that still spans the railroad tracks. From this bridge, five curvilinear streets branch out gracefully. Olmsted's drawings demonstrate the landscape architect's concern with the establishment of harmonious forms which are consistent with the fixed features of railroad right-of-way, approach roads, and the entry bridge.

Sudbrook Park was designed to offer all the amenities turn-of-the-century urbanites had come to value—large wooded lots, graceful homes on gently curving streets, green open spaces for community activities, and easy commuter access to downtown Baltimore.

The majority of Sudbrook's architecture dates from the mid-1890s to the mid-1910s. Gambrel roofs extending over a porch, polygonal towers, Palladian windows, and a profusion of bay windows are common architectural features. The house at the southeast corner of Sudbrook and Windsor Roads typifies the expansive domestic architecture of the turn of the century. Houses on Sudbrook Road, Cliveden, and Howard Streets also have fine examples of the architecture of this era.



CE93-3-FRE  
July 1993 344

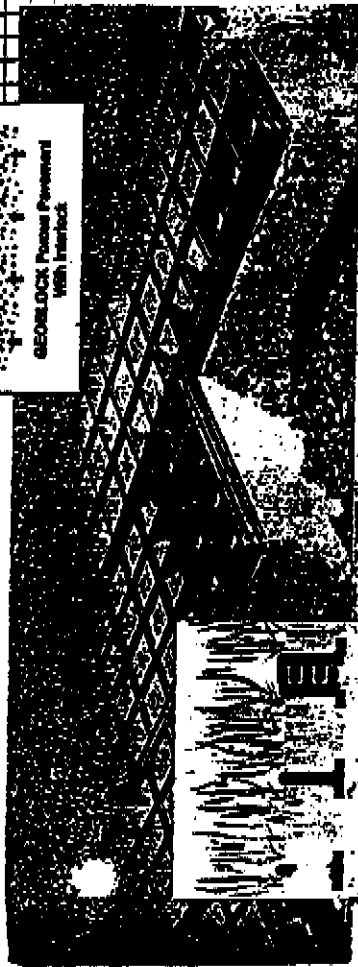
**GEOBLOCK**  
PERFORATED SYSTEM

Effect of Interlock  
Soil Stress Distribution



#### SUPPORTS HEAVY TRAFFIC AND CONCENTRATED LOADS

Geoblock porous pavement reduces the amount of site preparation and subgrade improvement required. Its torque-and-groove interlock forms a flexible structural bridge which spreads concentrated loads. The Geoblock system design flexes under loads that would break concrete. It assumes maximum load transfer from block to block thereby maintaining riding potential.



#### PROTECTS THE CROWN OF THE GRASS AND PREVENTS SOIL COMPACTION

Geoblock porous pavement directly supports the weight of pedestrian and vehicular traffic on its cell walls, protecting the crown of the grass and preventing over compaction and degradation of the topsoil. Geoblock porous pavement provides further support for healthy grass growth by permitting the use of a soil-and-sand or soil-and-gravel mixture for the subbase. The Geoblock system provides total design flexibility, since it allows a load bearing pavement to be located in areas where drainage and the physical flexibility of grass are desired.

#### REDUCES STORMWATER RUN-OFF AND ENHANCES GROUNDWATER RECHARGE

Geoblock porous pavement surface geometry reduces run-off, increases infiltration, resists erosion, and enhances groundwater recharge. 10% open area to the surface design allows water to percolate into the ground through drain holes in the bottom of the blocks.

## The GEOBLOCK system paves the way for better access lanes

Designed to handle your most demanding turf and load support needs, the Geoblock porous pavement system is a series of interlocking, high-strength blocks made from a minimum of 50% post-consumer recycled plastic.

The Geoblock system supports heavy or concentrated loads by creating a flexible structural bridge over a prepared subbase. As a result, you can use standard flexible pavement design procedures with it, just like you would with asphalt.

Installation is simplified because the Geoblock system requires less site preparation, less subgrade improvement, less excavation and less granular backfill than other porous pavement systems.

The Geoblock system installs easily around obstructions and corners and can be cut with ordinary hand tools or power tools. No formwork, cranes or concrete saws are required.

Installation time and money by eliminating the need for special tools, staples, cleats and rings. Plus, the Geoblock system's easy-to-handle size minimizes the quantity of blocks required on a given job.

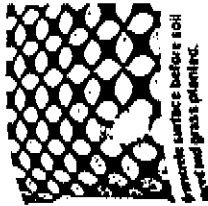
Manufactured with reinforced plastics, the Geoblock units offer excellent resistance to chemicals, fertilizers, temperature extremes and freeze-thaw cycles.

The Geoblock system is suitable for a wide range of turf protection and load bearing applications, including:

- emergency and utility access lanes
- auxiliary parking areas
- golf cart path shoulders and aprons
- driveways, driveway shoulders and medians
- highway medians, crossovers, waysides and shoulders
- pedestrian walkways and wheelchair access ways
- sidewalks and approaches to athletic fields
- applications to monuments, statues

© 1993 Geoblock Corp.

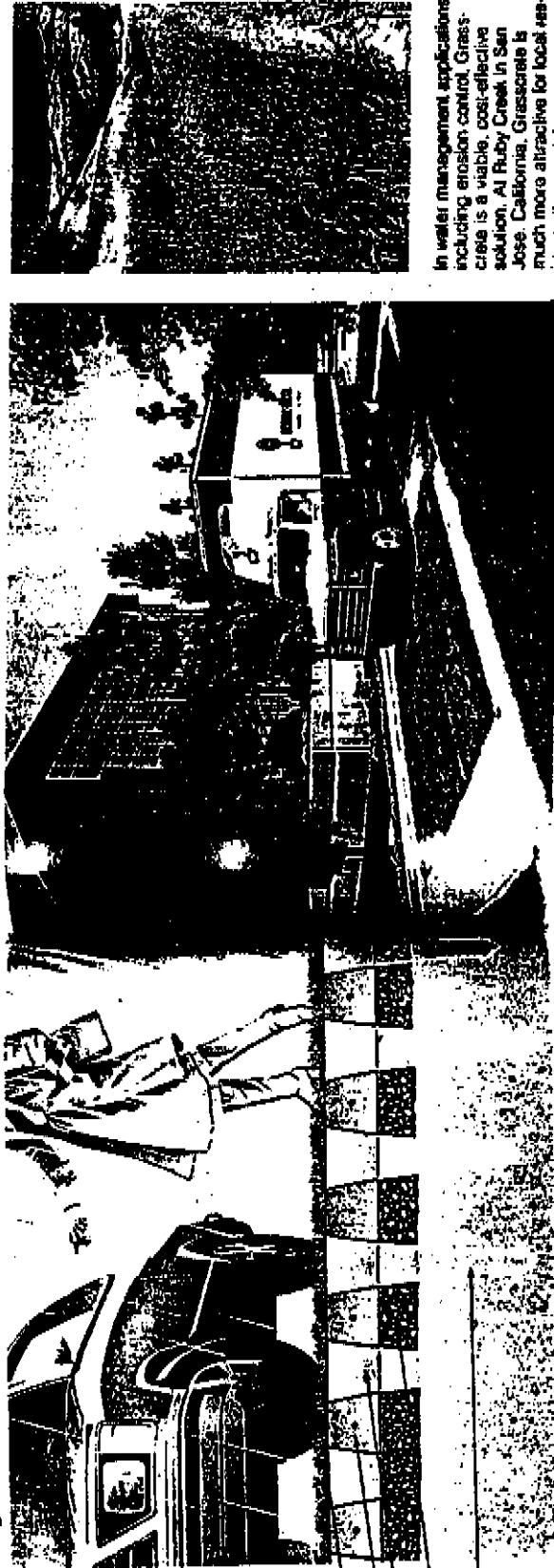
# Grasscrete



**CONTROLS EROSION**

**REDUCES STORMWATER RUNOFF  
MEETS GREENSPACE ZONING REQUIREMENTS**

## Cast-in-place, Monolithic, Continuous Reinforced Grass/Concrete Porous Pavement



Grasscrete offers a significant improvement over original concrete. Grasscrete is made of concrete slabs (instead of concrete slabs) and is used to create a new Grasscrete, resulting in a significant cost reduction and improved appearance. Grasscrete is competitively priced with other porous pavement products. It is long used for emergency access and parking, and is increasingly being used for erosion control, including stormwater runoff and landscape zoning.

Water Recharge

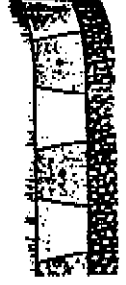
Grasscrete is a cast-in-place, monolithic, continuously reinforced grass/concrete porous pavement system. It can be used anywhere an impervious paving method is used, e.g. driveways, parking lots, access routes, embankments, drainage ditches, storage areas for heavy materials and vehicles etc.

Because of its structural integrity and continuous reinforcing, Grasscrete is not subject to differential settlement, which can occur with precast products.

Precast porous pavers are susceptible to differential settlement resulting in an uneven surface.



Grasscrete monolithic porous pavement stays flat under the heaviest traffic even when the sub-base is saturated with water.



In water management applications including erosion control, Grasscrete is a viable, cost-effective solution. At Ruby Creek in San Jose, California, Grasscrete is much more attractive for local residents than a plain concrete storm channel. Grasscrete allows infiltration while relieving hydrostatic pressure and doesn't require the subdrainage system necessary with solid concrete.

Grasscrete is a superb solution to problems created by the alteration of natural stormwater runoff patterns. By reducing runoff, Grasscrete actually recharges ground water recharge through infiltration, while improving the quality of the water supply and preventing erosion due to storms and floods. This "run-off source" pollution is the subject of an increasing number of state and local statutes. As the result of impervious surfaces in dense urban development, many state and local authorities are no longer allowing developers to increase the amount of runoff to sewer systems.

**TEST REPORTS AVAILABLE**

1. At least 65,000 lbs. Guernsey Face Truck-Ledder concrete MD 111 at 80 degrees with joints in buckets and no joints elsewhere. Saw was fully saturated. Expansion test per standards 200, 700 lbs. Maximum deflection: 0.015 in./ft.
2. Permeability Test. Differentiate and total amount found up to about 2 inches per hour. This is done through Grasscrete when it is installed. Contact samples with test results.

## Grasspave<sup>2</sup> Installs in Five Easy Steps

Grasspave<sup>2</sup> product can be installed in most applications at approximately 1000 (12:100 sq.) per hour, using these five installation steps.

### 1. Prepare porous base

Excavate depth of base course as determined by Engineer, place and compact sandy gravel base course material. To ensure the base is porous, run a hose and check that the water flows into the base and drains away. Add subsurface drainage as necessary to low spots.

### 2. Apply drogrow mix

Apply drogrow fertilizer and soil polymer mix over the base by hand or use a small fertilizer spreader set at 8 lbs. per 1000 (12:12 kg per 100 sq.) for seed, double for a seeded base. After raking lightly to distribute the mix vertically, re-compact the base. **DO NOT PUT TOPSOIL BETWEEN SANDY GRAVEL BASE AND GRASSPAVE UNITS!**

### 3. Lay Grasspave<sup>2</sup> units

Place Grasspave<sup>2</sup> units with rings up—directly over the sandy gravel base. Use pests and rings provided to interlock units. Units can be shaped easily with a grating shears or knife.

### 4. Seed or Sod Grasspave<sup>2</sup> units

- For seeding—fill to top of rings with clean sharp sand, then seed and mulch (hydro-mulch material is best).
- For thin sod—fill rings with clean sharp sand, then lay thin (1/2 inch) sod over area.

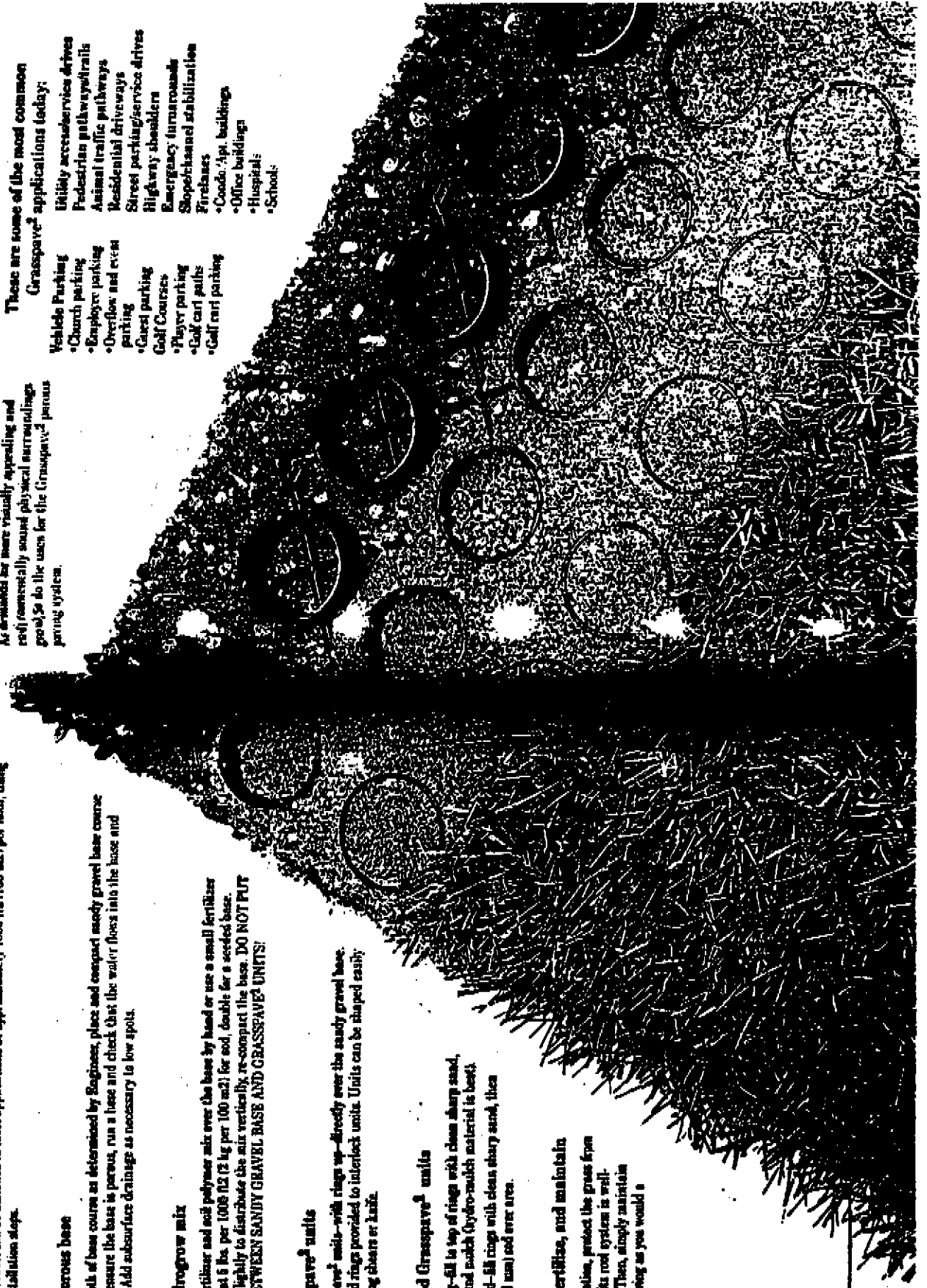
### 5. Irrigate, fertilize, and maintain

After establishment, protect the grass from traffic until its root system is well established. Then, simply maintain the grass paving as you would a grass lawn.

## A Growing List of Grasspave<sup>2</sup> Applications

As demands for more visually appealing and cost-effectively sound physical surroundings grow, so do the uses for the Grasspave<sup>2</sup> porous paving system.

- These are some of the most common Grasspave<sup>2</sup> applications today:
- Vehicle Parking**
    - Church parking
    - Employer parking
    - Overflow and event parking
    - Guard parking
    - Golf courses
    - Player parking
    - Golf cart paths
    - Golf cart parking
  - Utility access/service drives**
  - Pedestrian pathways/trails**
  - Animal traffic pathways**
  - Residential driveways**
  - Street parking/service drives**
  - Highway shoulders**
  - Emergency turnarounds**
  - Shops/classroom stabilization**
  - Firebases**
  - Coords. Apt. buildings
  - Office buildings
  - Hospitals
  - Schools



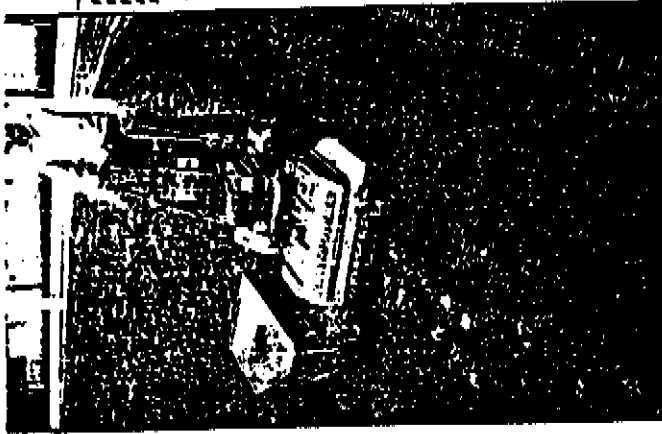
# CHECKER BLOCK®

Hastings CHECKER BLOCK gives you the strength of concrete in areas where you wish to maintain a grassy look including overflow parking, service roads, tree pits, and fire lanes. It is also ideal for stabilizing embankments along streams, rivers, and lakes.

CHECKER BLOCK offers a ready solution when used as a tree grating. It provides high stability while offering maximum water penetration.

CHECKER BLOCK consists of steel-reinforced, waffle-like configurations which create voids for grass or crushed stone. It features a greater ratio of grass to concrete than any similar material thereby improving aesthetic and environmental qualities.

MADE IN U.S.A.  
ALL THE CHECKER  
BLOCKS ARE CONCRETE  
AND REINFORCED WITH  
STEEL.



CHECKER BLOCK is manufactured in Southern California and Eastern Pennsylvania in order to facilitate shipments throughout the United States.

CHECKER BLOCK also enhances surface drainage allowing costly underground systems. Grass can easily be cut by standard lawn mowers.

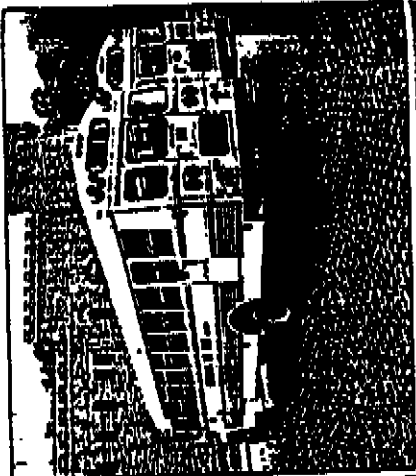
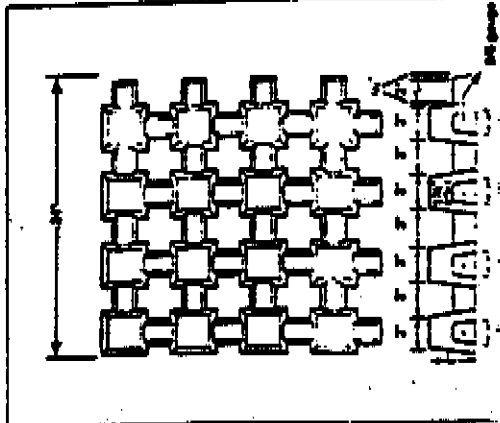
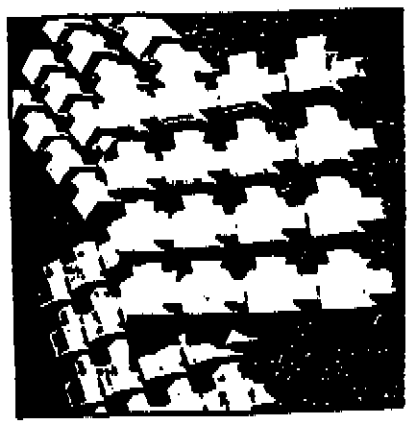


Photo courtesy of  
The Pennsylvania  
Department of  
Transportation

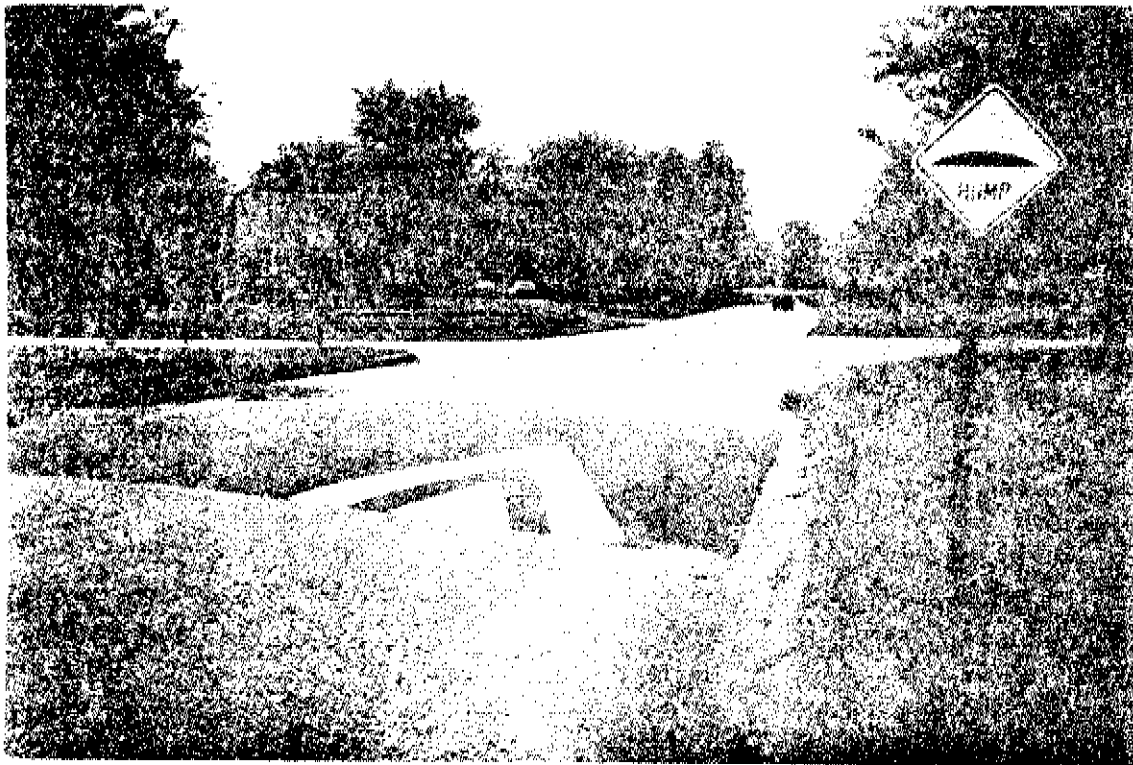


Photo courtesy of  
The Pennsylvania  
Department of  
Transportation





*Speedtables, Valley Mede, Howard County*



*Signage*



## APPENDIX J

**Sudbrook Park's Olmsted Heritage:  
Guidelines for Landscaping Public Spaces**

Although the community has not located Olmsted's specific planting plans for Sudbrook Park, it can obtain guidance in preserving and replenishing its Olmsted-designed landscape from Olmsted's letters to the Sudbrook Company, his writings and principles generally, and the landscaping description written by Edward Straka for Riverside, Illinois (Olmsted's first suburban village). The National Association for Olmsted Parks' Workbook Series, Vol. 3, "Defining an Ethic for Designed Landscapes" by Charles A. Birnbaum, ASLA, also provides relevant information. All of these sources were used in preparing these guidelines.

The land that became Sudbrook Park contained both wooded and open land. Olmsted wrote that the Sudbrook Company was to plant trees along the ten-foot planting spaces that bordered the roadways in the open areas (such as Cliveden Road); these trees were to be fifty feet apart and across from each other to provide a continuous umbrella of shade when mature. The graveled walk paths (either five or six-foot wide) were situated next to the planting strips in the open land, so they too would be shaded by the trees. In the wooded areas (along Windsor Road, for example), the order was reversed: the walk path was to be situated adjacent to the turf gutter with the planting strip beyond that. Trees were to be planted behind the path to supplement and appear a part of the existing forested area. Olmsted's cross sections for Sudbrook's roadways contain instructions on the contouring of the roads (which were sunk below grade to minimize their impact on the landscape), the planting strips and tree planting instructions, the walk paths and the formation of turf curbs and gutters.

Portions of Sudbrook Park were heavily wooded with oaks, hickory and chestnut trees in 1889. Most of the hickory and chestnut trees have been lost. Oaks still predominate on Windsor and Adana Roads. Norway maples predominate along Cliveden Road. But Sudbrook lost a large number of its majestic oaks and other mature trees during construction of the rapid transit through the community. The first summer after construction, Sudbrook again lost up to 80% of the new plantings because of a severe drought and theft. Moreover, many of Sudbrook Park's trees are old and have been lost through age or storms; routine replenishment must be continued to preserve the ambiance of the community (Sudbrook began this effort with its earlier participation in the Tree-mendous Maryland program). It takes forty or more years for an oak to reach a substantial size. Unless replenishment is increased and maintained on an annual basis, the setting so valued by residents may be lost.

Olmsted understood the power of naturalistic scenery and went to great lengths to create an overall landscape design that would provide a tranquil respite from the "cramped, confusing and controlled" conditions of the city. In his report on Riverside, he specified that each homeowner should be required to plant at least two trees between the house and road. He wanted no individual plant or single species to detract from the totality of his design; thus, he did not use or permit flowers or flower gardening plants to be used in his public spaces (gardens, if desired, were to be reserved for private rear and side yards). In an effort to beautify several of our public triangles, and before information about Olmsted's landscaping principles was known, individual residents planted flowering plants on several of the triangles; these should be removed to private yards as part of the comprehensive re-landscaping of Sudbrook Park's public spaces.

Olmsted's intent for Sudbrook was to create a rural, country-like setting. His curvilinear roads, slightly depressed below grade, blended unobtrusively into the adjacent landscape rather than detracting from it. Native plants were used whenever possible; the species had to be conducive to the natural scene. As noted by Straka in the Riverside guidelines:

Natural variation was established with unequal size and spacing of plant species, by varying sizes of green masses or groupings, and by contrasting open spaces with areas of plantings. Plants of different age and maturity were used to recreate natural progression and reforestation. Plant groups were formed by the use of multiples of like species and multiples of similar compatible species. The subtle variation in color tint and leaf texture enriched the grouping and accentuated the mass.

Varying heights of plants were used to create a layering effect to the landscape. Trees were intermixed with understory plantings to gently diminish the planting groups from their highest point down to the ground. Understory plantings were used to fill in or intensify the masses, to screen where desired, to form wind breaks, to create variety, and to show the rich quality of nature.

The overall quality of the landscape was the lush colors of "green" plantings supplemented by the cool "blue" of the sky . . . ; a landscape that was a variety of tints and textures of green foliage against a blue background.

Other points noted by Straka were that planting borders were to have curving, not straight, edges; trimming should be done only where absolutely necessary; the composition should be a unified and harmonious variety of landscape experiences; and exotic, spectacular, vivid colored or foreign items would disturb the impact of the landscape and were not to be used.

Site investigation and analysis, and plan development, are necessary prerequisites to appropriate site reconstruction. Thus, before beginning to re-landscape, certain preliminary steps are necessary.

#### Preliminary Steps:

1. Prepare an historic record documentation of plant materials in Sudbrook Park's public spaces, including all triangles, along the streetscape and in the Sudbrook Stream Valley Park. Recent research on Sudbrook's history and Olmsted design yielded some, but not extensive, information about pre-existing and selected plant materials used in early Sudbrook Park. This should be compiled for future reference. Any additionally discovered research should be added when found.
2. Survey and document existing trees and plant materials.
3. Determine appropriate preservation treatment in consultation with Olmsted scholars and landscape architects.
4. Create a preservation planting plan and plant list; every effort should be made to match the scale, form, and texture of the plant materials depicted in historic photographs or records that are representative of Olmsted's plan.

5. Site and install the selected plant materials; refer to Olmsted's directions for tree planting for Sudbrook. Replace with good topsoil any soil that is inferior.

The following guidelines are proposed to preserve, restore and enhance the Sudbrook Park landscape. When finalized and adopted, they are intended to be mandatory for Sudbrook's triangles and the Sudbrook Stream Valley Park, and recommended for residents with respect to their individual properties:

Landscaping Guidelines:<sup>1</sup>

1. Plant groups should contain an assemblage of hardwood trees and understory plantings which are:

a. Informal and naturalistic in appearance -- as if the grouping "just happened" and was not a conscious positioning of plants.

b. Asymmetrical in total form.

2. Individual plants should be subordinated to the whole group and not draw attention to themselves.

3. Generally, a multiple number of like species should be used within groups rather than using a single plant of a species. Group plantings that are a display of dissimilar single species are not part of the natural landscape.

4. Plants within a group should vary in size and height. This can be attained by using varying maturity plants of the same species, or by using compatible different species that by their natural growth habit form differing heights and widths, creating a stepped down effect. (Shrubs planted under and amongst low growing trees, and low growing trees under tall trees form this layering effect).

5. The edge of plant groups should be curved and/or serpentine rather than follow a straight line.

6. Plant groups should vary in size within the total landscape.

7. The ground of open, meadow-like areas and tree groves should be planted with grass.

8. The ground under groups of low branching trees or low shrubbery should be covered with ground cover.

9. Distracting items should not interfere with the total scene. Unusual, exotic or uniquely formed plants, flower beds and extremely unlike plants used in a group disrupt the tranquil and subtle, but powerful, concept intended by Olmsted.

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<sup>1</sup>These guidelines for Sudbrook are taken primarily from the guidelines prepared for Riverside by Edward Straka, which he generously shared with Sudbrook Park. Our thanks to him for the extensive work he did and for letting us adapt his guidelines to the Sudbrook landscape.

10. The overall color of the landscape should be the green of the plantings against the blue background of the sky. Red and yellow are not to be prevalent in the summer landscape.

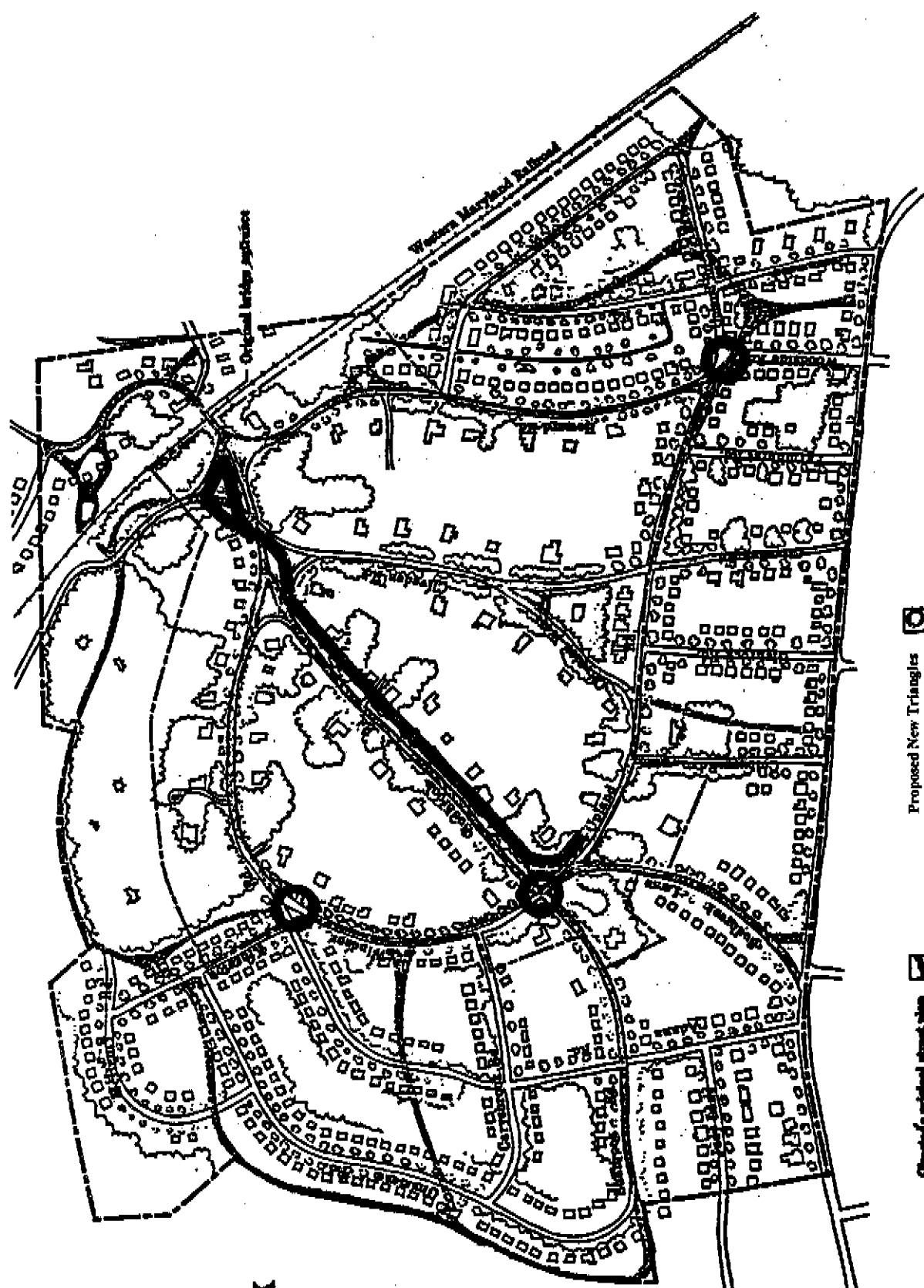
11. The materials, construction, and forms of all paths, benches, playground equipment, drinking fountains, stone property markers, lighting, historic markers, access roads, and other landscape elements should be governed by a naturalistic concept, being harmonious with and inconspicuous in the total landscape scene.

12. Formal planting is not a part of Sudbrook's public landscape concept, and the following formalistic characteristics should be avoided:

- a. Sculptured or exotic plants.
- b. Vividly colored plants and flowers.
- c. In line planting (species in a straight line).
- d. Geometric plant arrangements.
- e. Symmetrical grouping of plants.
- f. Equal height and size plants.
- g. Display planting (such as flower beds or gardens).
- h. Many dissimilar types of plants in a group, or the use of many single species within a group.
- i. Any plant or item that is so individualistic that it draws attention to itself.
- j. Plants marginally adaptive to the Sudbrook Park area.
- k. Use of stone or wood chip beds.
- l. Use of edge stripping material to border areas.

Understanding and implementing Olmsted's theory of landscape architecture in public spaces is essential to preserve the historic qualities of Sudbrook Park. Hopefully, residents will undertake this responsibility with enthusiasm.

# Sudbrook Park Proposed Triangles and Pedestrian Path



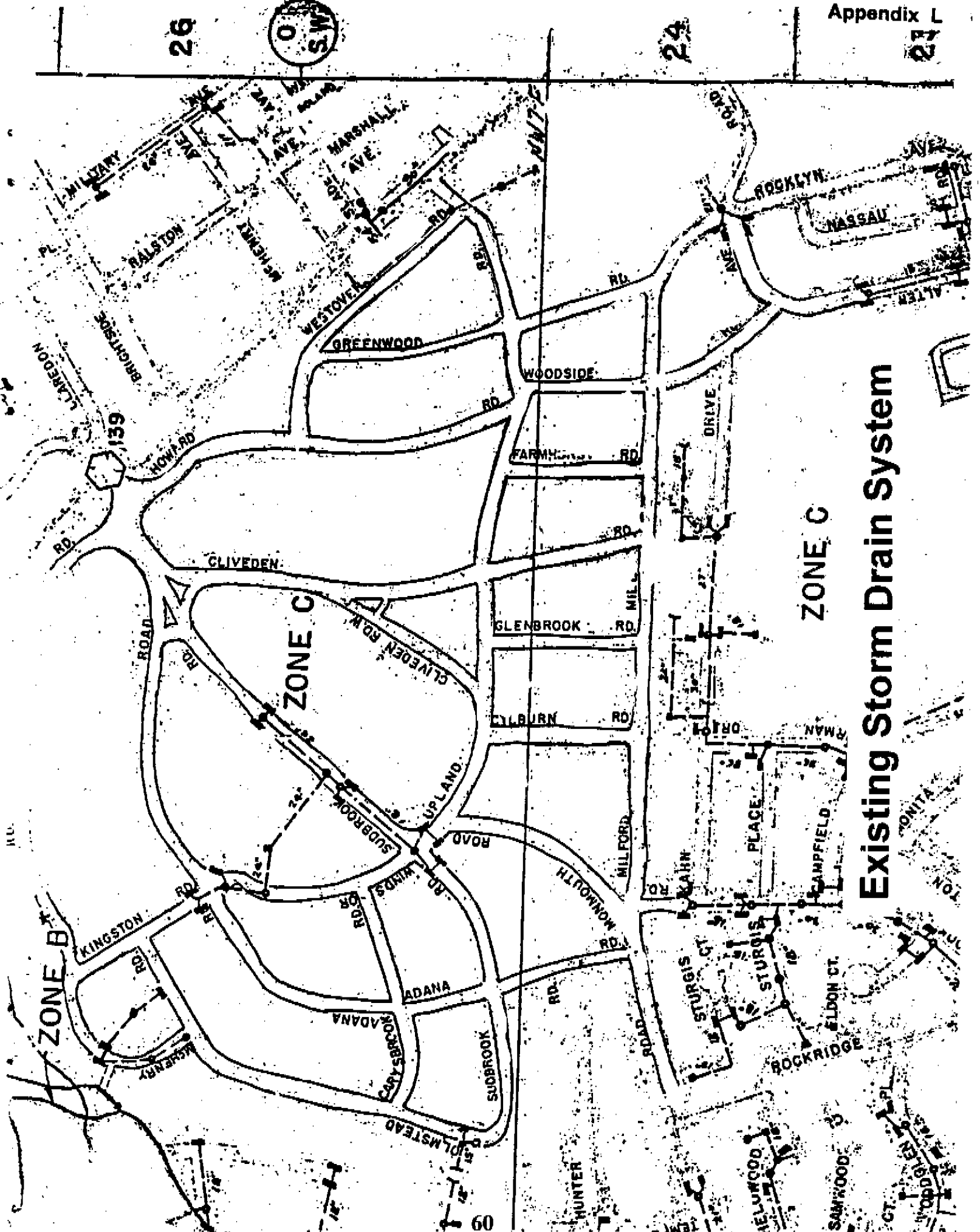
Planned Plan of  
**SUDBROOK**  
 Scale 1" = 200'  
 ↑

- Existing street junction
- Original street junction
- Proposed New Triangles
- Proposed Sudbrook Lane Pedestrian Path

26

29

27



# Existing Storm Drain System

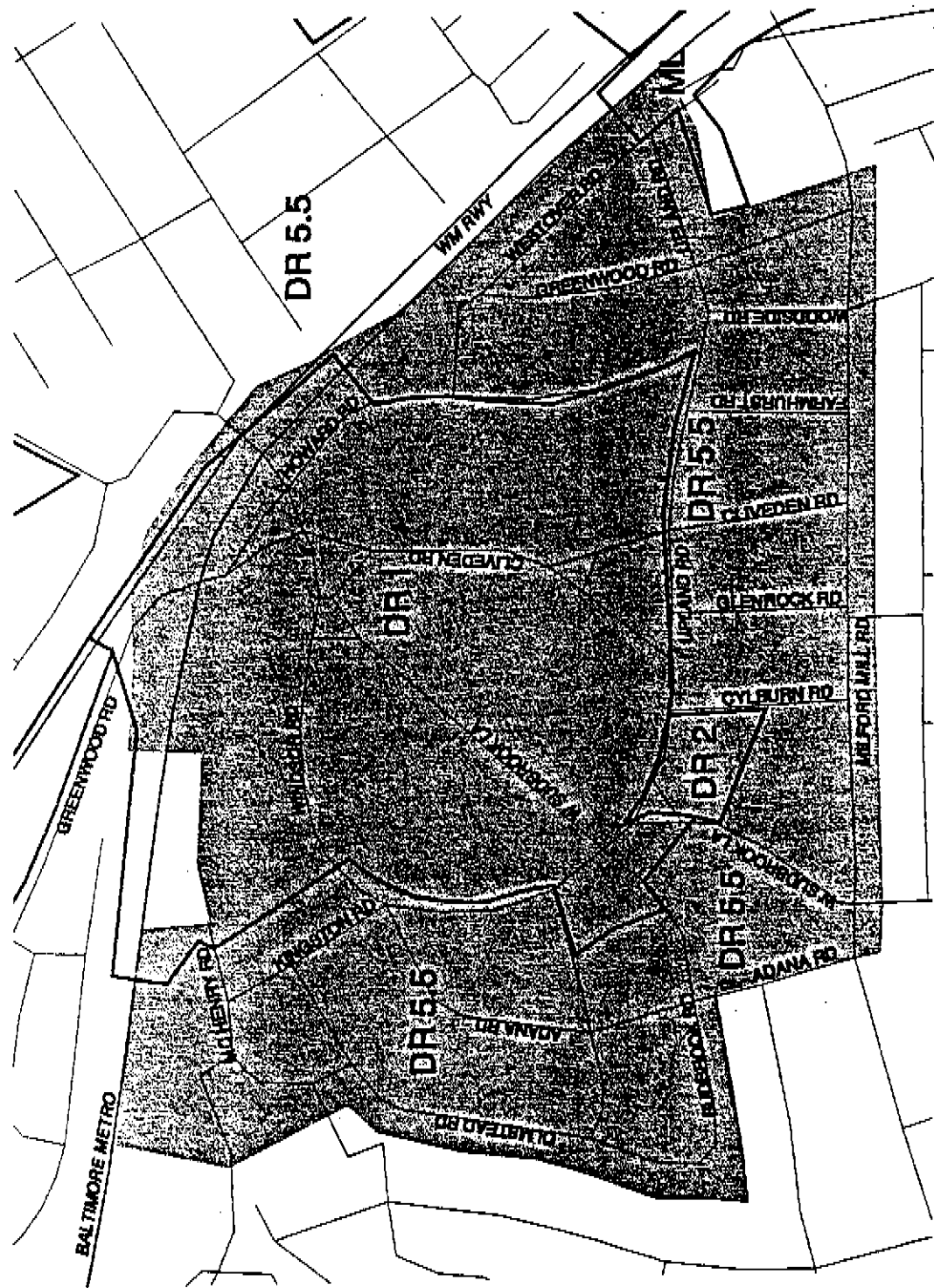
ZONE C

ZONE C

ZONE B

09

# Sudbrook Community Plan: Zoning



- Legend**
- Railroads
  - Roads
  - Sudbrook Area
  - Zoning

**Types of Zoning in the Sudbrook Area**  
 Density Residential:  
 DR 1 - 1 Density Unit per Acre  
 DR 2 - 2 Density Units per Acre  
 DR 5.5 - 5.5 Density Units Per Acre

Manufacturing:  
 ML - Manufacturing Light



City of Baltimore  
 Department of Planning  
 and Community Development

## APPENDIX N

612 Cliveden Road  
 Baltimore, MD 21208-4713  
 December 7, 1997

Individually, to each member of the School Board

This letter is on behalf of the Education Committee of Sudbrook Park, Inc. We have been working in the interest of all residents of Sudbrook Park for the past 18 months to support and strengthen our district's public schools. These include Bedford Elementary, Pikesville Middle and Milford Mill Academy. Our activities have involved both immediate action to market Bedford Elementary to parents of preschool children and long-range education planning as part of Sudbrook Park's Comprehensive Plan. A copy of the Public Schools and Education section of our draft neighborhood plan, submitted to the Baltimore County Planning Commission on November 17<sup>th</sup>, is attached.

Our Committee activities continue a long tradition of involvement at the PTA and district level. We are parents of current and past Bedford Elementary students and we witness a sharp decline in the number of Sudbrook Park parents who are sending their children to the districted elementary and high school. In fact, a disturbing number of residents place their homes on the market as their children approach both elementary and high school age. It is perceived that realtors are promoting Sudbrook Park as a "first house" neighborhood — or are not promoting it at all — due to the school situation. This elevates concerns about the stability of our neighborhood.

While we have, and will continue to do everything we can to erase perceptions of poor school quality that are sometimes based more on rumor than on fact, it is difficult to argue with parents concerned about low MSPAP scores. There are also concerns about the severe racial imbalances that exist at Bedford Elementary and Milford Mill Academy. Steps have been taken to enhance the academics at these schools, and performance has improved. Correction of racial imbalances will require redistricting. In recent discussion with County school administrators we have learned that, due to growth patterns in the Owings Mills area, some Northwest area schools will undergo redistricting. We ask that, as part of any such plan, Sudbrook Park's schools be given high priority. If Milford Mill Academy's academics and demographics cannot be changed dramatically, then we request that *Sudbrook Park be included within the Pikesville High School District*. If redistricting to achieve better racial balance is not feasible for Bedford Elementary, then *funding for increased staffing and/or a special enrichment program* (e.g., a magnet or an optional academic specialty program) should be provided at this school.

We would be happy to meet with you to discuss these concerns in more detail. Thank you for considering our request.

Sincerely,

Irma Frank

Education Committee Members

Mira Appleby  
 Steven Blizzard

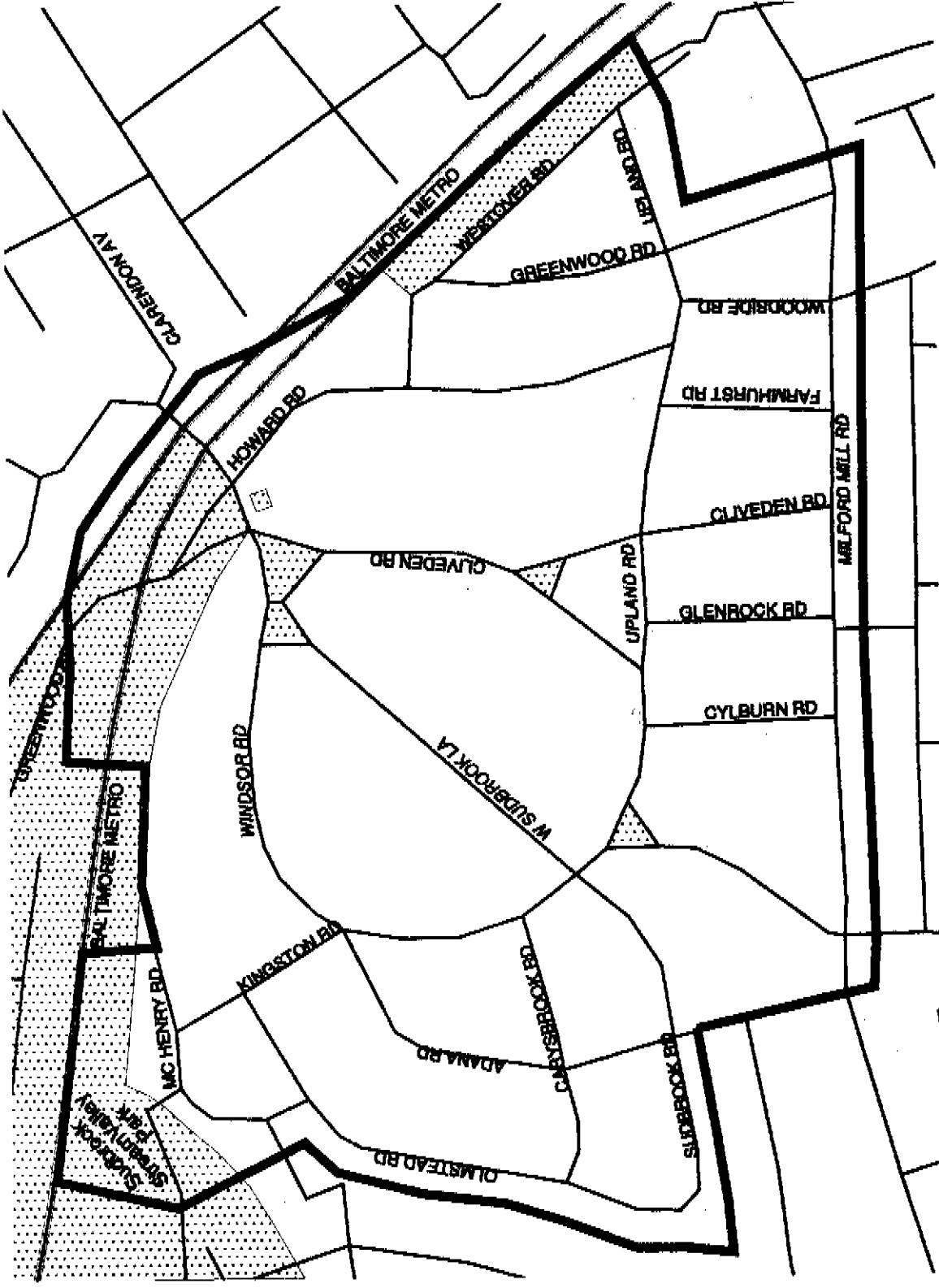
Karen Brown  
 Irma Frank, Chairperson

Richard Krohn  
 John Leith-Tetrault

Pat Leith-Tetrault  
 Myra Lewis



# Sudbrook Community Plan: Public Open Space



- Railroad
- Roads
- Public Open Space
- Sudbrook Area



Baltimore County  
Office of Planning  
County Courts Building  
401 Bosley Avenue  
Towson, Maryland 21204

*<http://www.co.ba.md.us>*